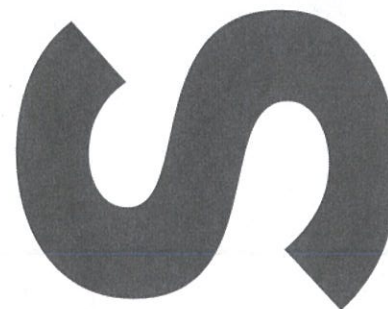
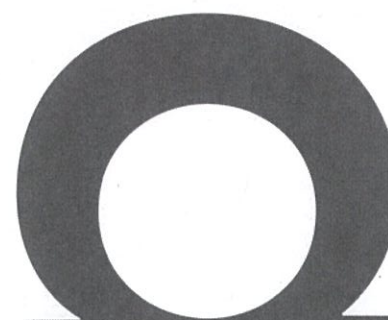


**Innovation in
Infrastructure**



**Echuca South East Rural Living Precinct
FINAL DRAFT Structure Plan**

July 2014

Prepared by Spiire Australia in association with TrafficWorks



This report has been prepared by the office of Spiire

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- **Ian Holmes – Trafficworks**
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Executive Summary

Campaspe Shire Council has for some time recognised the value of the Echuca South East Rural Living Precinct, and it's potential to make a meaningful contribution to the housing stock and choice available in Echuca and surrounds.

As such, Spiire Australia Pty Ltd was engaged by the Campaspe Shire Council to prepare a Structure Plan for the Echuca South East Rural Living Precinct.

The purpose of the Echuca South East Rural Living Precinct Structure Plan (the Structure Plan) is to give rightful recognition to the Precinct by setting parameters to guide its release and development. In doing so the Structure Plan has regard for the Precinct's many and varied attributes, and also considers the impacts on the land immediately surrounding the Precinct (the Investigation Area).

In preparing the Structure Plan, Spiire has actively engaged the landowners and community, services authorities and various State and local government departments.

In earlier phases of the consultation, the landowners and community expressed and overwhelming desire to retain the rural character of the Precinct while also allowing the opportunity for further development to occur.

The Structure Plan aims to achieve a balance between the desires of the community; managing a number of sensitive interface issues; enabling the Precinct to develop in an orderly and responsive manner; maintaining the integrity of the Echuca South East Industrial and Commercial Growth Corridor located to the west of the Precinct; and State government requirements.

Key recommendations of the Structure Plan include:

- Division of the Precinct into sub-areas, with specific requirements for the future development of these areas.
- Reduction in minimum lot sizes for subdivision of land.
- No formal sequencing of development
- Application of a Development Plan Overlay to seek appropriate consideration of, amongst other matters, flooding and drainage, and traffic management across the Precinct.
- Application of a Development Contributions Plan Overlay to ensure equitable levying for the provision of works, services and facilities required as a result of development.
- In some instances, the removal of legislative constraints
- Recognition and preservation of the integrity of the Echuca South East Industrial and Commercial Corridor.



The proposed minimum lot size for subdivision differs throughout the Precinct and this Report must be read in conjunction with the structure plan included in Section 6. The recommendations and implementation methods have been made with consideration for the needs of the community, the Council as well as the requirements of State Government.

1. Introduction

1.1 Purpose

Campaspe Shire Council has for some time recognised the value of the Echuca South East Rural Living Precinct, and its potential to make a meaningful contribution to the housing stock and choice available in Echuca and surrounds.

The recent introduction by the State Government of a significantly reduced default minimum lot size of 2 hectares for land within the Rural Living Zone has provided the opportunity for the Echuca South East Rural Living Precinct to come “alive”. Currently the minimum lot size for the area is 8 hectares.

The purpose of the Echuca South East Rural Living Precinct Structure Plan (the Structure Plan) is to give rightful recognition to this Precinct by setting parameters to guide its release and development. In doing so the Structure Plan has regard for the Precinct’s many and varied attributes, and also considers the impacts on the land immediately surrounding the Precinct (the investigation area).

Specifically, the objectives of the Structure Plan are to:

- Provide lot diversity and mixed housing opportunities taking into consideration the objectives of the Rural Living Zone;
- Consolidate rural residential and lifestyle development on land currently zoned Rural Living;
- Reflect site, environmental, locational, access and servicing constraints;
- Identify preferred timing, and staging of development;
- Provide the data, strategic justification and analytical basis for planning scheme amendments to give statutory effect to the Structure Plan.

In considering the lot density and layout of future development, consideration needs to be given to a number of constraints. These include, but are not limited to:

- Section 173 Agreements preventing further subdivision within the Precinct
- Flooding and inundation
- Lack of services including reticulated water, sewerage, gas and electricity.
- Current road network and potentially dangerous intersections along the Murray Valley Highway.

Consideration must also be given to the Precinct’s close proximity to the South East Industrial and Commercial Growth Corridor. Such consideration must give regard to the potential impacts of the Structure Plan on the overall integrity of this Corridor.

Future development within the Precinct and/or Investigation Area must also have consideration of other potential environmental issues, as well as the continued agricultural use of the Farming Zone land surrounding the Precinct.

1.2 Precinct

The Precinct occupies approximately 830 hectares of rural living zoned land and its surrounds, as shown on the map below, and is located approximately 5 kilometres south east of the Echuca town centre. It is generally bound by the Murray Valley Highway to the north, Mackenzie Road to the west, Kelsh Road to the south and Simmie Road to the east.

Figure 1 below shows the Precinct (shaded area) and Investigation Area (dashed line)

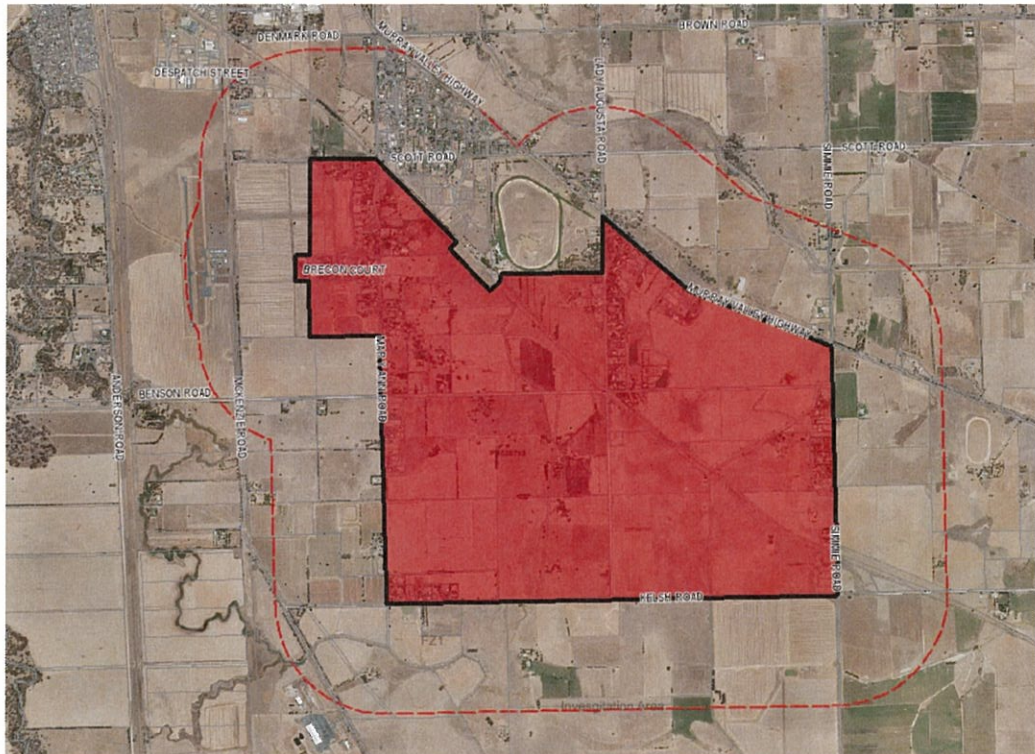


Figure 1: Precinct and Investigation Area Location

Within the Precinct there are approximately 400 properties. Based on the number of dwellings in the area (approximately 250) and the average occupancy rate for Echuca of 2.4 persons per household, the overall population of the Precinct is approximately 600 people.

2. Precinct Analysis and Influencing Conditions

2.1 Land Use and Character

As shown on Figure 2 below, land in the Precinct is dominated by two main land uses – rural residential/living and agriculture.

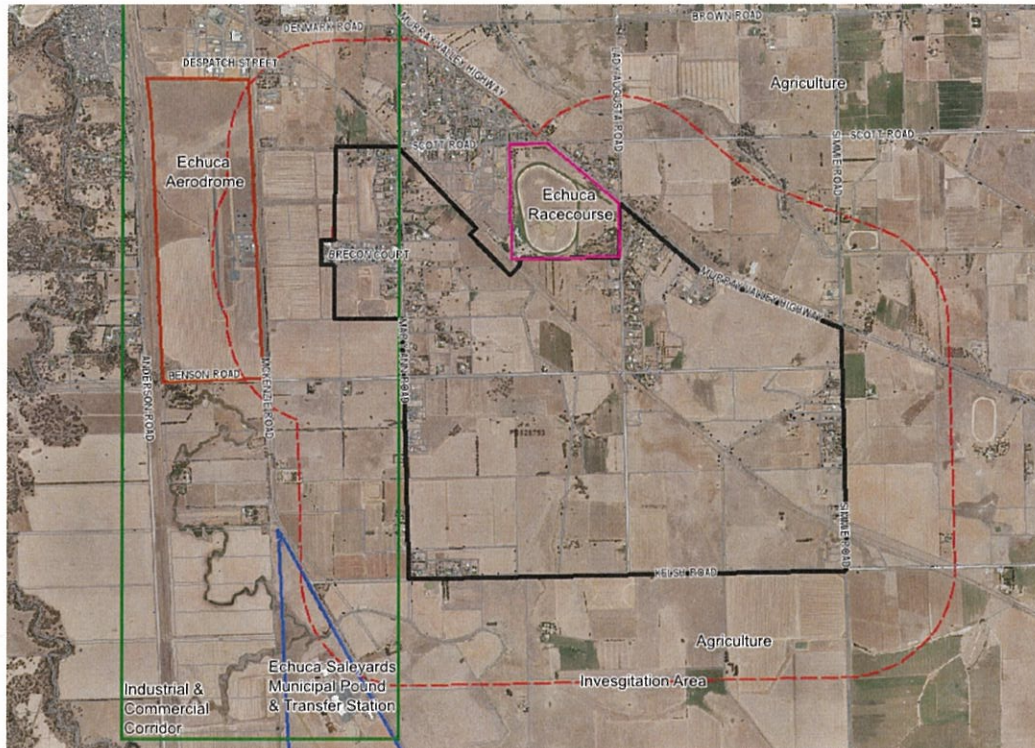


Figure 2: Precinct and Investigation Area Land Use Plan

Rural residential development is concentrated in linear clusters along the main roads within the Precinct, where access to these lots has been readily available. Clusters are located along both sides of Mary Ann Road south of Scott Road and including Brecon Court, the eastern side of Mary Ann Road south of Benson Road, along Kelsh Road near its intersection with MaryAnn Road, the western side of Simmie Road between Kelsh and Benson Roads, and along Lady Augusta Road between the Murray Valley Highway and Benson Road.

Generally dwellings are single storey and contain associated shedding and other typical rural residential infrastructure. A small number of existing lots are developed with more than one dwelling or are used for tourism/accommodation.

The majority of these rural living allotments are approximately 0.6 hectare to 1 hectare in area. Prior to the introduction of the current Rural Living Zone in September 2013, subdivision that created lots that were less than the minimum size were permitted, on the condition the developer entered into an agreement with the Campaspe Shire

Council under Section 173 of the Planning and Environment Act 1987 which prevented any further subdivision of the balance of the land. This has resulted in substantial amounts of land, often referred to as “balance lots” within the Precinct unable to be developed more intensively. Figure 3 below highlights, in red, the extent of land which is encumbered by these agreements. This is a significant impediment to the future appropriate development of the Precinct.

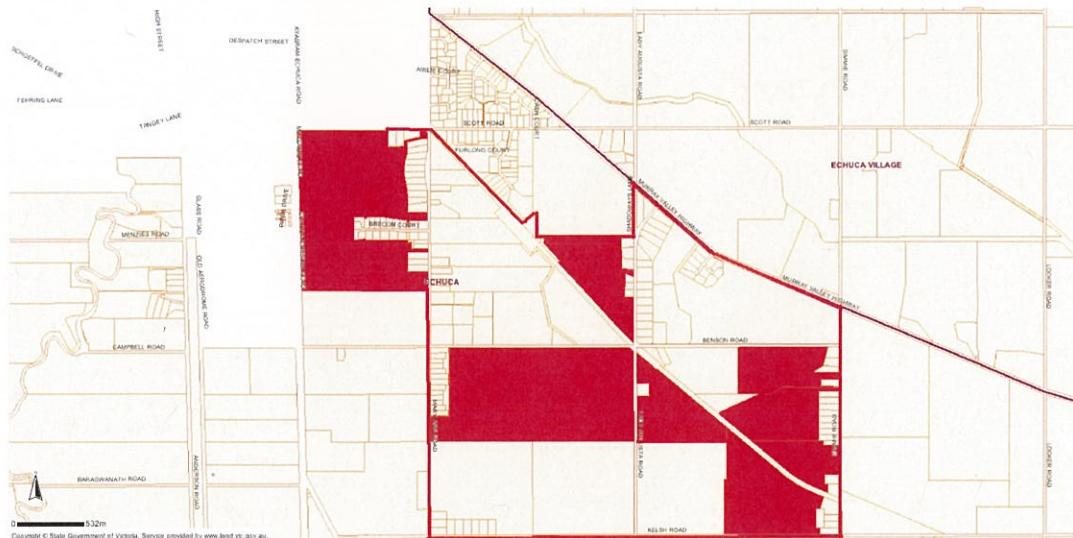


Figure 3: Land affected by Section 173 Agreement

The use of land for agriculture within the Precinct is more concentrated to the southern areas, generally south of Benson Road. In the past, this land has been used for a range of agricultural land uses, including dairy and cropping, and still has access to water for irrigation purposes. Agricultural uses mainly now include cropping.

Also influencing the character of the Precinct is the varied land uses which share interfaces with the Precinct. As shown in the aerial photo in Figure 2, these include the nearby Echuca South East Industrial and Commercial Growth to the west, the Echuca Aerodrome also to the west, the Municipal Saleyards and Waste Transfer Station to the south west, farming land to the south and east, the Echuca Racecourse, an area of Low Density Residential Zone and the Murray Valley Highway to the north. A more detailed discussion regarding these interface areas follows below.

2.2 Interfaces

The combined area of the Precinct and Investigation Area is geographically similar in area to the urban area of Echuca. As such there are a number of interfaces which need to be considered as part of planning the Precinct's future.

North Western Interface

The north western section of the Precinct is closest to the urban area of Echuca. While this area is the closest to the amenities of the town, it is also bound on its western side by existing industrial land that forms part of the recognised and endorsed Echuca South East Industrial and Commercial Growth Corridor. A number of food manufacturing businesses are located just north of the Precinct in Denmark Road. These businesses and factories generate significant employment, both directly and indirectly, and support the significant agricultural base of the municipality. This Corridor is discussed in more detail in Section 3.5 of this Report.

Western and South Western Interfaces

The land along the western boundary of the Precinct is also within this identified Industrial and Commercial Growth Corridor, which extends south to the existing Waste Transfer Station, Echuca Saleyards and Municipal Pound, all of which are located to the immediate south west of the Precinct. Within this area to the west of the Precinct there are a small number of dwellings and agricultural enterprises which interface with the Precinct.

Also to the west of the Precinct is the Echuca Aerodrome. The main operational area to the aerodrome is located on the west side of McKenzie Road, however there is a portion of land on the east side of McKenzie Road which is included in the investigation area. According to the Echuca Aerodrome Master Plan (Beca, 2010) this eastern area is surplus to the operational needs of the aerodrome and may have potential for open space or other community facilities. The Aerodrome is discussed further in Section 2.3 of this Report.

Southern and Eastern Interfaces

Land to the south and east of the Precinct is primarily used for agricultural activities. It too has a small number of dwellings scattered throughout this area, most of which are farm dwellings associated with the management and operations of the farming properties upon which they are located. There are also a number of dwellings that are isolated rural residential properties and not necessarily associated with farms.

Northern Interface

North of the Investigation Area, on the opposite side of the Murray Valley Highway is Echuca Village, which is developed with a small number of dwellings on lots generally greater than 1 hectare in area. This land is substantially constrained by flooding associated with its proximity to the Murray River, which is located just to the north. Echuca Village had previously been identified to accommodate the future residential growth of Echuca, however further studies revealed the land was significantly constrained by flooding. The utilisation of this area for traditional residential development has since been abandoned and relocated to the western side of Echuca.

North of the Precinct and within the Investigation Area is the Echuca Racecourse and an area of land within the Low Density Residential Zone (LDRZ). The land within the LDRZ is used for residential purposes and is fully committed for development.

2.3 Surrounding Land Uses

Echuca Aerodrome

The Echuca Aerodrome is located on the western edge of the Precinct on the opposite side of McKenzie Road. Rural airports are a critical piece of infrastructure for regional towns and cities as they provide an increased level of connectivity to other areas that may not necessarily be achieved by other forms of transport. This connectivity has a range of benefits economic, safety, health and transport.

The Echuca Aerodrome Master Plan – Part B: The Master Plan (Beca: 2010) has identified that the aerodrome has adequate capacity for the next 10-15 years. It has been further identified that the Echuca Aerodrome is in a strategic location in terms of hangar space as Metropolitan Airports are at hangar capacity and come at a considerable cost. The next ring of hangars in Victoria's regional cities are expected to reach capacity within the next seven years and costs are increasing in these centres. The current plan for the Echuca Aerodrome has identified that there will be an increase in demand for the facilities at this facility.

Whilst further development of the current facility for commuter services has been deemed to be financially unviable at this stage, it has been identified by the users of the aerodrome that short term expansion to have a greater amount of hangar space for storage as well as opening up the facility for more general aviation use is required.

The Master Plan identifies the need for the importance of the facility to be included in the local policy within the Campaspe Planning Scheme. The Echuca Aerodrome has been identified as a significant asset for the economic development of the Shire of Campaspe through Planning Scheme Amendment C76 which was gazetted on 23 June 2011.

The location of the airport presents a number of opportunities and constraints for the town, the Precinct and the aerodrome itself. Its proximity to the urban area of Echuca means that it is better able to be utilised by the community and business, rather than being isolated and underutilised as has occurred in other towns throughout the state. While the location is advantageous in terms of its useability, any development surrounding the area needs to be considerate of the needs and future use of the airport. Consideration needs to be given not only to the built form within the flight path areas of the aerodrome but also to the amenity impacts of the operation of the aerodrome on surrounding sensitive uses.

Echuca Racecourse

The Echuca Racecourse is located within the Precinct, on the Murray Valley Highway and north of the Echuca-Toolamba Railway Line. The racecourse has 11 TAB racing meetings per year, with the Melbourne Cup day meeting and two-day Echuca Cup Carnival over the Labour Day weekend in March being the feature events. Future expansion of the racecourse would logically be to the south towards the railway line, however any potential expansion of the existing racecourse will need to be considered in the context of the Precinct and abutting land uses and expectations.

Echuca Saleyards, Waste Transfer Station and Municipal Pound

The Echuca Saleyards, Waste Transfer Station and Municipal Pound are located at the south western corner of the investigation area on McKenzie Road and form the southern edge of the Industrial and Commercial Growth Corridor. These facilities are located approximately 750 metres from the Precinct, with the closest properties being on the corner of Mary Ann and Kelsh Roads. The use of these facilities varies from daily to weekly.

2.4 Zoning Provisions

As shown in Figure 4 below, the Precinct is within the Rural Living Zone, with the exception of the Echuca – Toolamba Railway, which is owned by Vic Track and within the Public Use Zone. The Investigation Area includes a variety of zones, a description of these follows.

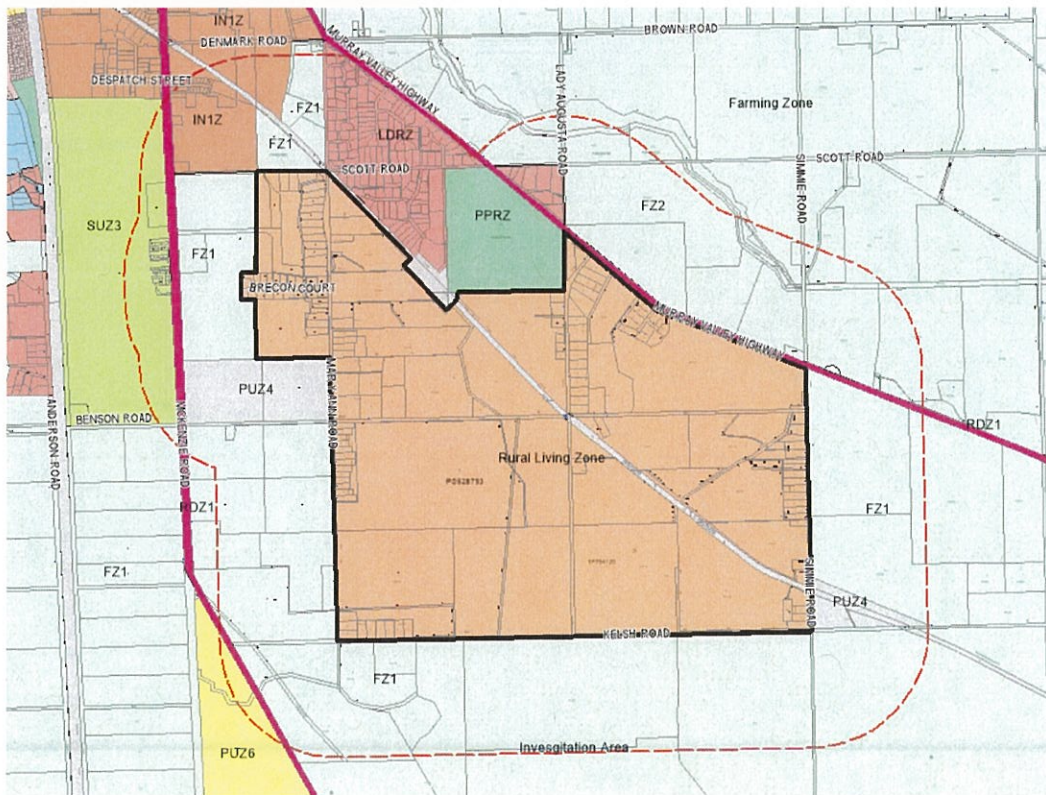


Figure 4: Zoning Map

Rural Living Zone

The Rural Living Zone forms part of the rural suite of zones. This zone applies to the majority of the Precinct, and provides for agricultural activities which may include a dwelling. Lots within this zone are of a significantly larger size than traditional residential lots to cater for different lifestyle requirements, such as the keeping of animals. The State default minimum lot size for subdivision is 2 hectares, however the Schedule to the Rural Living Zone as specified in the Campaspe Planning Scheme currently specifies a minimum lot size of 8 hectares.

Farming Zone

The Farming Zone essentially provides and encourages land for agricultural use. The development of land within this zone is generally required to provide some support or benefit to an agricultural land use, whether it be existing or proposed, however Planning Practice Note No. 42 outlines other scenarios for which the application of the Farming Zone is appropriate. This zone also allows for some forms of rural industry. Any proposed development within this zone must have consideration for any surrounding land uses, including residential. Under the provisions of the Campaspe Planning Scheme, there are two schedules to the Farming Zone. Schedule 1 (FZ1) applies to land surrounding the Precinct to the east, west and south while Schedule 2 (FZ2) applies to the north. Land in FZ1 has a minimum lot size of 60 hectares and the minimum lot size for use of land for a dwelling is 80 hectares. Within FZ2, the minimum lot sizes of 40 hectares for both subdivision and use of land for a dwelling (without a planning permit). Both schedules recognise the high quality agricultural land in the Shire of Campaspe.

Low Density Residential Zone

The Low Density Residential Zone provides primarily for residential land use on lots generally of approximately 4,000m² without connection to reticulated sewer, or 2,000m² with connection. Within the Investigation Area the low density residential lots are provided with reticulated water but rely on septic systems for the on-site treatment and disposal of waste water.

Public Park and Recreation Zone

The Echuca Racecourse is subject to this zone. It is generally applied to publicly owned land which is used for recreational purposes.

Industrial 1 Zone

Land in the Industrial 1 Zone has been identified for industrial land uses including manufacturing and ancillary land uses such as convenience shops and warehousing. The Echuca South East Industrial and Commercial Corridor is located to the west of the site, with the northern portion of this Corridor being zoned Industrial 1 Zone to reflect current industry, including the major food manufacturing precinct located in Denmark Road, as well as providing zoned land ready for development.

Public Use Zone

The Public Use Zone is applied to the Echuca – Toolamba Railway Line and to land west of McKenzie Road which was set aside for the Echuca Aerodrome but is currently surplus to its operations.

2.5 Overlay Provisions

Given some of the physical attributes and various land uses within the Precinct and Investigation Area, five different overlays affect the Precinct and Investigation Area, as shown in a combined manner in Figure 5 and described below.

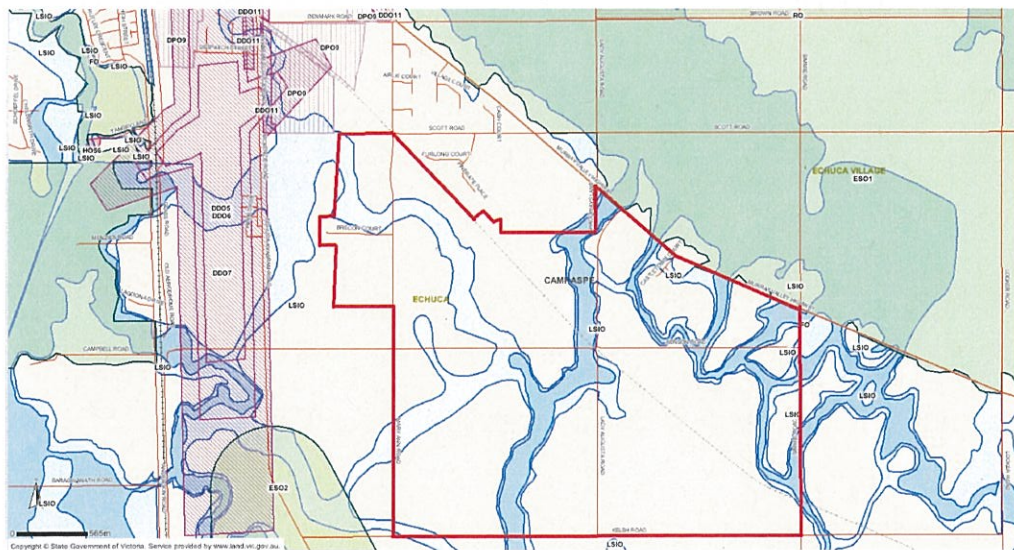


Figure 5: Overlay Map

Land Subject to Inundation Overlay and Floodway Overlay

These two overlays relate to planning for flood events, and protecting the integrity of the floodways. Land in the Land Subject to Inundation Overlay has the potential to be inundated with water in a flood event while land in the Floodway Overlay provides an overland flow path for water in the event of a flood. Generally the development of land affected by these overlays requires approval from the relevant catchment management authority under the provisions of the Planning and Environment Act 1987.

Environmental Significance Overlay

This overlay applies to protect significant environments, with these 'environments' being be natural or physical. This overlay applies to land shaded green in Figure 5, both areas located within vicinity of the Investigation Area. ESO 1 applies to the Murray River corridor and seeks to appropriately recognise, plan for and manage the Murray River. ESO 2 applies to land outside the Precinct and around the Echuca and District Livestock Exchange, Municipal Pound and Waste Transfer Station and seeks to minimise conflict between potentially conflicting land uses.

Development Plan Overlay

The Development Plan Overlay generally applies to a number of properties and requires a co-ordinated plan to be prepared for the future of a specific area generally before further development can occur. Once the development plan has been approved, any future development must be generally in accordance with the plan. The Development Plan Overlay is shown in purple shading in Figure 5 and applies to the Echuca South East Industrial Land.

Design and Development Overlay

The Design and Development Overlay is applied where Council intends to protect and enhance an environment and encourage development that complies with the character of a place. The Design and Development Overlay is shown in purple shading in Figure 5 and applies to the environs of the Echuca Aerodrome

2.6 Servicing and Infrastructure

The Precinct and Investigation Area are supplied, in part, with reticulated water. Electricity and telecommunications are available, while reticulated sewerage and gas are unavailable at this time. This is discussed further in Section 5 of this report.

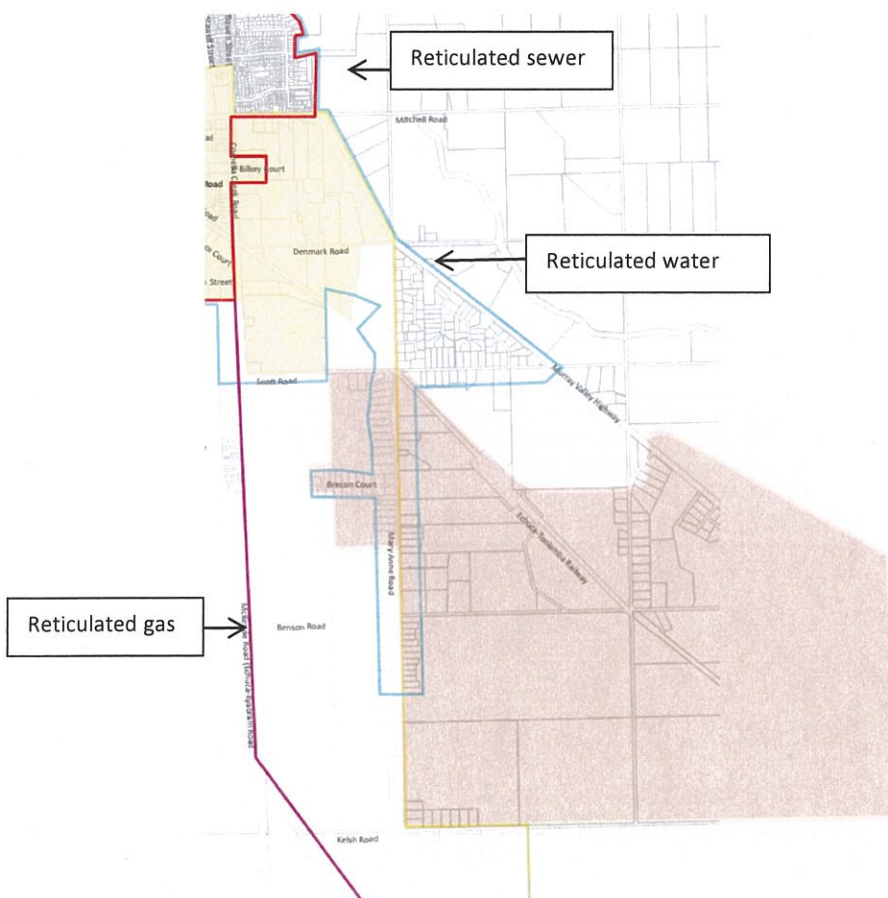


Figure 6: Location of Physical Infrastructure Services (Source: Echuca South East Commercial and Industrial Growth Corridor Land Strategy, 2011)

2.7 Access and Circulation

The Precinct's road network consists of a grid pattern of east west and north south oriented two lane two way roads. These roads allow for traffic to travel through and within the Precinct, and connect it with other roads which provide access to localities external to the Precinct. The grid pattern roads are supported by a small number of courts which provide access to dwellings.

The majority of the grid roads, including Mary Ann Road, Lady Augusta Road, Scott Road, Benson Road and Kelsh Road are classed as Access Roads¹ whilst Simmie Road is classed as a Collector Road.

The courts, including Brecon Court, Castletown Court and Foxdale Court are Access Roads.

The external roads in the near vicinity which connect the Precinct to other locations include the Murray Valley Highway, Echuca-Kyabram Road (also known as McKenzie Road and Cornelia Creek Road) and Denmark Road and Looker Road.

The Murray Valley Highway and Echuca-Kyabram Road are Arterial Roads for which VicRoads is the Responsible Authority. The Campaspe Shire Council is the Responsible Authority for all other roads in and near the Precinct.

Other than the Arterial Roads, the only road which has a function to carry traffic through the Precinct is Simmie Road. However, it has been noted that Kelsh Road (via Looker Road) appears to be carrying some through traffic as it is part of a route which links the Murray Valley Highway to the east with the new Echuca Livestock Exchange site on the Echuca-Kyabram Road to the southwest of the Precinct.

In the Precinct and the abutting low density residential area north of Scott Road it is noted that courts providing access to residences from the grid roads are not linked with each other for traffic connectivity, nor for pedestrian and cycling movements.

Traffic Flows

Precinct Roads

Existing traffic volumes on roads within the Precinct range between less than 30vpd in the southeast corner of the Precinct (on Kelsh Road west of Simmie Road) and approximately 650vpd in the northern section of the Precinct (on Mary Anne Road at Scott Road). It is further estimated that Mary Anne Road carries approximately 1,200vpd on its approach to Denmark Road. Traffic volumes on other roads in Precinct range between these values. Modelling of the traffic generated by the Precinct is shown in Attachment 1. The Precinct roads are suitable for carrying the current volumes of traffic.

Murray Valley Highway

The Murray Valley Highway is currently carrying approximately 4,200vpd along the northeast frontage of the RLZ Precinct. AM and PM peak hour volumes are 8% of daily flow. These traffic flows are well below volumes when provision of additional capacity, .e.g. highway duplication, would be considered. North of Denmark Road the highway is estimated to carry 4,800vpd (4,200vpd plus 600vpd from Mary Ann Road)

Echuca-Kyabram Road

The Echuca-Kyabram Road is currently carrying approximately 1,800vpd to the west of the Precinct. AM and PM peak hour volumes are 9% of daily flow. North of Denmark Road it is estimated to be carrying 2,000vpd.

Traffic Generation

The traffic volumes generated by existing residences in and near the Precinct is 9.5 trips per residence per day with peak hour generation of 0.9 and 1.0 trips per hour in the AM and PM peak hours respectively.

Traffic Distribution

Traffic to and from the Precinct is predominantly oriented to Echuca. It is estimated the distribution of traffic is in the order of 90% Echuca oriented with the remaining 10% travelling to other locations such as to the east along the Murray Valley Highway and to the south along Echuca-Kyabram Road.

It is estimated that there are currently 150 residences in the Precinct and 95 residences in the area north of Scott Road and other locations abutting the Precinct. These areas are generating approximately 1,500 and 950 vehicle trips per day respectively.

It is noted that traffic travelling along Mary Ann Road can choose to travel to / from Echuca via the Murray valley Highway or Denmark Road and Echuca Kyabram Road. The highway route is approximately 300m shorter than travel via Denmark Road and is estimated to be in the order of 50 seconds quicker. The choice of route is a personal decision which could be expected to take into account a driver's attitude to safety at the Denmark Road / Murray Valley Highway intersection and the value of savings from using the shortest and quickest route. A 50 / 50 split in choice between the two options is adopted for this report.

The approximate distribution of traffic is shown in Table 1 over the page. This Table is based on 100% of traffic being Echuca oriented.

ROAD	LOCATION	TRAFFIC FROM RLZ PRECINCT (vpd)	TRAFFIC FROM AREA NORTH OF SCOTT ROAD (vpd)	TOTAL ¹ (vpd)
Kelsh Road	East of Mary Ann Road	80	20	100
Benson Rod	East of Mary Ann Road	30	0	30
Mary Ann Road	South of Scott Road	650	0	650
Mary Ann Road	South of Denmark Road	700	450	1,150
Scott Road	West of Murray Valley Highway	20	130	150
Lady Augusta Road	South of Murray Valley Highway	300	0	300
Simmie Road	South of Murray Valley Highway	250	0	250
Murray Valley Highway	South of Denmark Road	650	450	1,100
Murray Valley Highway	North of Denmark Road	1,000	700	1,700
Denmark Road	West of Mary Ann Road	350	200	550
Echuca-Kyabram Road	South of Denmark Road	150	50	200
Echuca-Kyabram Road	North of Denmark Road	500	300	800

Table 1: Estimated distribution of traffic from existing residences in the RLZ Precinct and residences north of Scott Road (totals rounded to 50vpd)

Note 1: Total rounded to 50vpd.

Road Conditions

All roads within and near the Precinct are two way roads and generally have two sealed traffic lanes. The exceptions are the unsealed roads which are generally carrying low traffic volume and are not along the frontages of residential lots. Examples of the unsealed roads are:

- Kelsh Road, other than a 500m section east of Mary Ann Road.
- Lady Augusta Road, south of Benson Road,
- Benson Road, other than some short sections along residence frontages,
- Scott Road, west of Mary Ann Road.

Typical examples of the roads within the Precinct are shown in Appendix 1.

Speed Zones

The default rural speed limit of 100km/h applies to the majority of roads within and near the Precinct. Exceptions to this speed limit are:

- The 70km/h Area speed limit which includes road such at Mary Ann Road (north of Benson Road), Scott Road Brecon Court.
- 80km/h speed zone on Lady Augusta Road north of Benson Road.
- 60km/h and 80km/h speed zones at passive controlled rail crossings on Mary Ann Road and Lady Augusta Road.
- 90km/h speed zone on the Murray Valley Highway between north of Denmark Road and Scott Road.
- 80km/h speed zone on Denmark Road.
- 60km/h on Echuca-Kyabram Road from south of Denmark Road north to the Murray Valley Highway in Echuca.

Intersection Treatments

Intersections within the Precinct are generally consistent with basic rural intersection layouts for low traffic roads, i.e. they are not provided with additional lanes or road widening to assist vehicles to turn safely from through roads to side roads.

On roads in the near vicinity of the Precinct intersections are also consistent with typical rural intersections with some improvements to the through road which reduce the risk of casualty crashes involving turning vehicles occurring. The improvements include:

- Sealed shoulders along the Murray Valley Highway.
- Widening of the Murray Valley Highway's eastbound traffic lanes to allow eastbound vehicles to pass to the left of vehicles making right turns at the Wallingford Court and Castletown Court intersections
- A left turn slip lane at the Echuca-Kyabram Road / Denmark Road intersection.
- Left and right turn lanes at the Echuca-Kyabram Road / Despatch Road intersection.
- Intersection improvements in accordance with the potential treatments included in Appendix 1 to this document.

There are five rail level crossings within or near the Precinct. The level crossings on Mary Ann Road, Lady Augusta Road, Simmie Road and Kelsh Road are passive controlled crossings, and the crossing on Echuca-Kyabram Road has flashing lights control. The level crossings on Mary Ann Road and Lady Augusta Road are unusual and more complex than typical low use rural level crossings as the rail line runs diagonally through cross road intersections, i.e. at the Mary Ann Road / Scott Road and Lady Augusta Road / Benson Road intersections. Photos of these intersections can be seen at Appendix 1

Crash History

There have been three casualty crashes within and eight casualty crashes in the near vicinity of the Precinct during the 5 year period January 2008 to December 2012.

The crashes within the Precinct had the following key aspects:

- Two crashes occurred on Mary Ann Road and one on Simmie Road.
- Each crash involved a single vehicle, each with a driver or rider only.
- One crash resulted in serious injuries to the driver and two crashes resulted in minor injuries to the driver / rider.
- Two crashes involved the vehicle running off the road into a hazard, details of the third crash are unclear.
- One crash occurred in each of a 60km/h, 80km/h and 100km/h speed zone.
- Two crashes occurred at day time, one at night time.
- Two crashes occurred in dry conditions, one in muddy conditions.

Roads within the Precinct have a road safety record that is considered to be reasonable and does not justify road safety improvement works in the foreseeable future.

The casualty crashes in the near vicinity of the Precinct consist of:

- Six crashes on the Murray Valley Highway between Denmark Road and Simmie Road.
- One crash on Denmark Road.
- One crash on Echuca-Kyabram Road.

The crashes on the Murray Valley Highway had the following key aspects:

- Three crashes at intersections, one at each of the intersections with Scott Road, Lady Augusta Road and Simmie Road.
- Two crashes at very short distances north of side roads, i.e. Denmark Road and Scott Road.
- One crash mid way between the intersections with Scott Road and Lady Augusta Road.
- Five of the six casualty crashes were or are likely to have been intersection operation related.
- Two crashes resulted in serious injuries and four resulted in minor injuries to the driver / rider.

The Murray Valley Highway's road safety history in the vicinity of the Precinct is sufficiently high to justify consideration of road safety improvement works in the near future.

The crashes on Echuca-Kyabram Road and Denmark Road had the following key aspects:

- The crash at the Echuca-Kyabram Road / Kelsh Road intersection occurred at day time and in dry conditions and resulted in minor injuries.
- The crash on Denmark Road involved a vehicle and a cyclist colliding at a driveway. It occurred at day time and in dry conditions and resulted in serious injuries.

The road safety record on these two roads is considered to be reasonable and does not justify road safety improvement works in the foreseeable future.

It can be concluded that, from a road safety record perspective, any improvement works at or near Precinct access road intersections along the Murray Valley Highway should incorporate road safety improvements.

Cycle/Pedestrian Access

There are no dedicated cycle paths within the Precinct, therefore cyclists are currently required to utilise the existing road network. There are however two informal cycle routes along the edge of the Precinct. One runs along Denmark Road across the Murray Valley Highway through to Brown Road. The other is located along the length of Simmie Road. Both of these routes are utilised by recreational cyclists.

Similar to the above, there are no dedicated pedestrian paths within the Precinct, with pedestrians utilising the existing road network.

Public Transport

No community wide public transport is currently available within the Precinct. Several school bus routes traverse the Precinct, providing access for school children into Echuca.

The Echuca-Toolamba Railway line runs diagonally through the Precinct in an east west direction, however is currently used for freight only.

Echuca Toolamba Railway Line

The Echuca Toolamba Railway Line was recently re-opened following a long period of closure. This railway line has been identified in the Department of Transport, Planning and Local Infrastructure's plans for long term strategy to improve freight efficiency and better connect Victoria both internally and externally. The plan, *Victoria – The Freight State*, identifies the significant investment in the last 18 months to upgrade and reopen the Echuca Toolamba Railway Line to form part of the state freight network which services export markets, as well as internal freight demands.

3. Background Literature Review

It is acknowledged there is a wealth of background literature relevant to the Precinct, however the reviews below provide a summary of the key documents relevant to the preparation of the structure plan for this Precinct and investigation area.

3.1 Echuca Low Density Residential and Rural Living Land Review, RPD Group, June 2004

The Echuca Low Density Residential and Rural Living Land Review was commissioned by the Campaspe Shire Council to provide a strategy that continued to support low density residential and rural living development in and around Echuca. The project sought to provide:

- An understanding of the market for low density residential or rural residential property in and around Echuca (including Moama);
- An assessment of the suitability and level of supply of existing low density residential or rural residential property precincts; and
- A strategy for sustainable low density residential or rural residential development as a component of residential growth.

The Review focused on the three existing low density residential/rural living areas, including the Precinct subject to this Report, and assessed the sustainability of their ongoing development.

Key findings of the Review relevant to the Precinct included;

- *Expansion of low density residential development to other areas around Echuca is limited by flooding constraints, conventional residential growth plans and industrial growth plans;*
- *There was a broad statement based on advice from Council's Economic Development Unit that:*
 - *none of the investigation areas were considered to be of high agricultural quality; and*
 - *although some areas are irrigated, the infrastructure is nearing the end of its economic life;*
- *Lack of reticulated water and sewer in the southeast was considered a constraint on the extent/intensity of development in that area;*
- *Strong population growth and building activity is predicted to continue.*
- *Rural residential and low density residential development is a significant and popular component of the local residential property market.*
- *The supply and demand analysis (up to 2002) indicated:*
 - *The supply of existing low density residential and rural living lots to 2ha (86 lots) would be developed by 2006.*

- *At maximum subdivision potential (based on present planning controls) all available low density residential zoned land would be developed by around 2013.*
- *The Southeast rural living area was not expected to be fully developed for at least 12 years (2015). However, the level of demand in this area would increase if the RLZ lot averaging provisions were used to produce smaller lots;*
- *The analysis may understate land requirements, as there could be pent up demand due to the limited supply in Wharparilla Drive and Fehring Lane;*
- *There is stronger demand for low density housing on lots that are smaller than the 4,000m² minimum allowed under the LDRZ;*
- *The existing planning controls in the Southeast area (Rural Living) were not considered to be consistent with demand. The resulting development has been ad hoc and does not reflect preferred development patterns; and*
- *Opportunities in a number of different locations are important to accommodate different buyers. The amenity and level of services in the rural residential precincts are reflected in land prices with Wharparilla Drive demanding the highest prices, followed by the Fehring Lane area, then the Southeast area.*

The Review recognised low density residential development as a legitimate and ongoing element of Echuca's housing options. The relevant recommended strategy involves:

- *Extension of Low Density Residential zoning in each of the study precincts to provide a choice of location and development type; In this regard the Review recommended extending the low density development opportunities south of Scott Road.*
- *The focus for future low density residential growth being to the north west (ie Wharparilla Drive area);*
- *A mix of lot sizes;*
- *Limiting the existing oversupply of rural living land in the southeast area as this area has experienced limited development, is comparatively peripheral and is a less desirable housing form; In this regard the Review recommended the area south of Benson Road and east of Lady Augusta Road be 'back zoned' to the Rural Zone (now Farming Zone).*
- *Protection of other urban growth opportunities, particularly industrial operation and expansion adjacent to the southeast area; In this regard the Review recommended buffer areas be retained as Rural (now Farming) or at the most rezone to Rural Living but prevent further subdivision, and ensure appropriate dwelling setbacks are maintained, and*
- *Managing the release of land in each of the three areas to ensure that supply does not exceed the estimated demand for a 10-year period.*

3.2 Amendment C34 to Campaspe Planning Scheme, PPV Report, November 2005

Amendment C34 proposed to amend the Schedule to the Rural Living Zone to allow land bound by Lady Augusta Road, the Echuca Racecourse and the Echuca-Tatura Railway to be subdivided into lots with a minimum size of 0.5 hectares and an average lots size of 1.0 hectare. The amendment also proposed to apply a Development Plan Overlay to the land to restrict the development of lots to uses associated with the Echuca Racecourse.

The primary strategic justification for the amendment was to provide for racecourse related residential uses but the Independent Panel considering the amendment was not convinced there was a real likelihood that such uses would eventuate. The Panel found more typical rural living involving keeping horses could be accommodated in the wider Echuca area without providing for more intensive development of this land. The Panel concluded that Amendment C34 should be abandoned.

3.3 Amendment C35 to Campaspe Planning Scheme, PPV Report, November 2005

Amendment C35 proposed to rezone land adjoining Mary Ann and Scott Roads from Rural Living Zone to Low Density Residential Zone and apply a Development Plan Overlay.

In considering the amendment, the Independent Panel recommended the area of land to be rezoned be reduced to be contained within an area bound by Mary Ann Road, Murray Valley Highway, the Echuca Racecourse and the Railway Line. It was identified that although the amendment was not sufficiently supported by existing policy within the planning scheme, the rezoning would result in the consolidation of ad-hoc development that had occurred previously within the area. The Panel also recommended the MSS be strengthened to clearly indicate that requests for further rezonings to facilitate rural residential development must demonstrate that the supply of land for this purpose will not exceed projected needs for a 10 year planning timeframe for Echuca overall nor in a low density residential precinct.

3.4 Amendment C36 to Campaspe Planning Scheme, PPV Report, November 2005

Amendment C36 applies to approximately 226 hectares of land about 3 kilometres west of Echuca. It proposed to rezone this land to Residential 1 Zone and Low Density Residential Zone, yielding a potential for up to 426 lots. The Panel supported this amendment subject to conditions.

3.5 Echuca South East Industrial and Commercial Growth Corridor Strategy – January 2011 (Centrum Town Planning, with CPG Australia and Terraco)

The Echuca South East Industrial and Commercial Growth Corridor Strategy provides a vision for the future development of the corridor over the next 20-30 years. Specifically, it provides an *'industrial and commercial land strategy' which identifies areas of land for the long term development of large, medium and small scale industries; structure plans identifying areas specifically for large, medium and small scale industrial development and commercial (trade supplies) opportunities; design guidelines for private land adjacent to main roads; and an implementation plan outlining actions, timeframes and responsibilities to guide the implementation of the Strategy.*

The Strategy identifies that Echuca currently has approximately 125 hectares of vacant land zoned for industrial purposes. The south east growth corridor contains most of Echuca's industrial and commercial land, with large undeveloped parcels located in the vicinity of Denmark Road and Despatch Street/Old Aerodrome Road.

Research undertaken for the Strategy found that there is likely to be consistent demand for industrial land in Echuca in the foreseeable future associated with projected population growth. It also found that there is likely to be sufficient zoned land available to satisfy demand associated with projected population growth for the Shire to at least 2026.

Prior to the Strategy, Council had not had the benefit of a comprehensive long term strategic plan to guide commercial and industrial development in the Echuca urban area and safeguard opportunities for the future expansion of the urban area to the south. The study has been prepared in response to the emergence of a number of complex land use planning and infrastructure issues in the investigation area. Resolution of these issues requires a precinct-wide planning approach. Key planning issues in the south east growth corridor include:

- *rural living development located in close proximity to areas with potential for longterm industrial growth;*
- *limited supply of industrial lots for industries that require buffers of more than 500 metres from residential areas;*
- *little recognition, or protection of, the important food manufacturing precinct in the Planning Scheme*
- *the need to protect the long-term integrity of important public uses including the Echuca Aerodrome, Shire of Campaspe saleyards, pound and waste transfer station;*
- *extensive areas of land that are subject to inundation in major flood events;*
- *key parcels of vacant land and land with redevelopment potential in the Ogilvie Avenue area;*
- *lack of consistent approaches to built form, signage and parking and access arrangements in the existing urban area;*
- *lack of consistent landscape treatments on private landholdings and in the public realm.*

The Strategy has a range of specific objectives and implementation tasks to addresses these issues, including a 'Structure Plan' and a 'Strategic Framework Plan' to guide future land use planning decisions to the south of the existing urban area.

Of particular relevance to the Precinct, the strategy recommended the following objectives and strategies:

- *Apply the Industrial 3 Zone to industrial land abutting or in close proximity to residential areas to encourage compatible industrial uses and enable consideration of amenity impacts through the planning permit process.*
- *Protect the food manufacturing precinct from the encroachment of sensitive or noncompatible uses.*
- *Discourage the development of dwellings in the Farming Zone on the west side of Mary Anne Road and on the east side of the Murray Valley Highway opposite the food precinct.*
- *Give notice of planning applications for sensitive uses in the area surrounding the food precinct to the owners and operators of land in the food precinct.*
- *Protect existing rural living uses from unreasonable amenity impacts from industry*

- *Maintain a buffer of at least 100 metres from existing rural living development to any new industrial lots, with roads, drainage or similar uses allowed within the buffer.*
- *Retain part of surplus aerodrome land at the east of McKenzie Road (precinct 6) as a buffer to rural living development and use this land for public, recreation or community uses, as shown on the Strategic Framework Plan (Figure 8).*

3.6 Regional Rural Land Use Strategy, October 2008 and August 2010

The RRLUS was prepared as a strategy to secure and promote the future of agriculture across the region through modifications to the Campaspe Planning Scheme, the Greater Shepparton Planning Scheme and the Moira Planning Scheme.

The RRLUS was implemented into the Campaspe Planning Scheme through Amendment C69, which had a Gazettal date of 19 September 2013. The amendment applied to all land within the Farming Zone and the Rural Conservation Zone. The amendment proposed a number of changes including designating Farming Zone areas into one of three new categories, introducing the Rural Activity Zone, applying an Environment Significance Overlay to the Murray River environs and making a number of changes to Campaspe local planning policies.

3.7 Echuca Housing Strategy, Beca Pty Ltd, March 2011

The Echuca Housing Strategy was completed in 2011 and provides a framework for residential growth within the town. This study directs greenfield development and other standard residential development to the west of the town and relates only to the current residential zones in the Victoria Planning Provisions. This strategy does not discuss rural living development nor direct any future standard residential development in either the Precinct or Investigation Area.

3.8 Amendment C86 to Campaspe Planning Scheme, PPV Report, September 2013

Amendment C86 to the Campaspe Planning Scheme sought to implement the recommendations of a number of adopted strategic reports including the Echuca Housing Strategy and the Echuca South East Industrial and Commercial Growth Corridor Strategy. A number of objections were received, including objections in regard to the recommendations of the Echuca South East Industrial and Commercial Growth Corridor Strategy.

In summary and in relation to the Industrial and Commercial Strategy, the Independent Panel hearing submissions in regard to C86 recommended

- *Amending the boundaries of Environmental Significance Overlay – Schedule 2 to restrict the area covered by the Overlay to a 500 metre buffer surrounding the Council's existing saleyard, pound and waste transfer station.*
- *Amend the reference to the Mary Ann Road area in the Echuca Town Structure Plan 3 to read as follows: 'Rural land between low density residential and industrial land, subject to future investigation'.*

The Panel also made the following recommendation for further work:

- *Council, as quickly as practical, should prepare and consult on a program of work leading to the preparation and adoption of a structure plan for the northern section of the Echuca south east industrial and commercial growth corridor including, in particular, the extent and buffer requirements for the food precinct and the contentious interface areas between industrial and residential uses.*

It is noted that during the course of the Hearing, Council representatives advised the Panel that, given the significant number of objections received to the recommendation to rezone land on the west side of Mary Ann Road from Farming Zone to industrial 3 Zone, it had decided to retain the status quo and the current reference to rural zoning of the land i.e. notation on the Echuca Town Structure Plan 3 to read: '*Rural buffer between low density residential and industrial land*'.

3.9 Urban Development Program Regional Residential Report: Campaspe, Spatial Economics, September 2013

This Urban Development Program Report was undertaken by Spatial Economics Pty Ltd, and commissioned by the Department of Transport, Planning and Local Infrastructure.

Residential building activity

As measured from July 2006 to July 2012 residential building approval activity within the Shire of Campaspe has averaged 162 per annum. The vast majority of building approvals (90%) since July 2006 have been separate houses, the remaining 10% for medium density dwellings.

Residential lot construction

From July 2006 to December 2012, on average there were 145 residential lots created. The majority (52%) were broad hectare/major infill lots, followed by minor infill lot construction at 36% and 12% rural residential. The majority (64%) of residential lot construction activity was located within Echuca, followed by Kyabram (22%), and Rochester (7%).

Rural Residential lot construction activity as measured from July 2006 to December 2012 across the Shire has averaged 18 lots per annum. This represents 12% of all residential lot construction activity across the municipality.

Of this lot construction activity – 74% was zoned Low Density Residential (LDRZ) and 26% Rural Living (RLZ). The majority of this subdivision activity was located in the suburbs of Echuca and Kyabram.

Residential supply

As at December 2009 across the Shire there was a total lot stock of rural residential allotments of 1,221. Of this stock, 217 lots were vacant, a lot vacancy rate of 18%.

By zone type, as at December 2009 there were 585 Low Density Residential (LDRZ) allotments, of which 61 were vacant across the municipality, a lot vacancy of 10%. In comparison, there were a total of 636 Rural Living (RLZ) zoned allotments, of which 156 were vacant – a lot vacancy rate of 25%.

The location of the majority of rural residential lots across the municipality includes:

- Echuca - total 523 lots (lot vacancy of 10%);
- Kyabram - total 211 lots (lot vacancy of 10%);
- Rushworth - total 179 lots (lot vacancy of 33%);
- Toolleen - total 119 lots (lot vacancy of 38%); and
- Waranga Shores - total 125 lots (lot vacancy of 22%).

Future rural residential (LDRZ) unzoned areas have been identified. In summary, a total of 40.4 hectares of future rural residential land stocks have been identified. However, 28 hectares are located in the township areas of Kyabram and 12 hectares in Rochester.

Of interest is the finding that within the Shire, there is an estimated lot potential within Future Residential areas of approximately 6,192. Of this lot potential by township/urban area:

- 6,020 lots are located in Echuca;
- 100 lots in Waranga Shores; and
- 72 lots in Gunbower.

Projected Demand

Projected dwelling requirements sourced from Victoria In Future 2012 indicate that from 2011 to 2031 there will be a total dwelling requirement of 4,609 (an annual average of 230 dwellings). For specific time cohorts average annual dwelling requirements include:

- 2011 to 2016 - 178;
- 2016 to 2021 - 232;
- 2021 to 2026 - 262 and
- 2026 to 2031 - 250.

As measured from 2011 to 2031, the average annual projected demand by Statistical Local Area is:

- Echuca: 146 dwellings per annum (Urban area of Echuca);
- Kyabram: 43 dwellings per annum (e.g. Tongala, Kyabram, Koyoga);
- Rochester: 30 dwellings per annum (e.g. Rochester, Gunbower); and
- South: 12 dwellings per annum (e.g. Stanhope, Rushworth).

An alternative demand projection based on recent (2006 to 2012) building approval activity in conjunction with growth rates identified in the State Governments' projections, results in a 20% (940 dwellings) decrease in total dwelling requirements from 2011 to 2031.

Years of Supply

Overall it is estimated, based on the identified supply and Victoria in Future 2012 demand projections, there is over 15 years total supply of residential land stocks across the Shire of Campaspe.

Based on the Victoria in Future and alternative demand projections as described above, there is sufficient land zoned land to accommodate standard residential development and major infill development within the Echuca Statistical Local Area for the next 6 years and 8 years respectively.

3.10 Urban Development Program Regional Industrial Report: Campaspe, Spatial Economics, September 2013

This Urban Development Program Report was undertaken by Spatial Economics Pty Ltd, and commissioned by the Department of Transport, Planning and Local Infrastructure.

Industrial Supply

As at December 2012, there was a total of 474 hectares zoned industrial land stock, of which 209 hectares were assessed as available (supply) for industrial purpose development - a total land area vacancy rate of 44%.

In terms of the geographic spread of zoned industrial land stocks across the Shire of Campaspe nearly half of the industrial land is located in Echuca (229 hectares). Tongala has 20% (97 hectares) of the industrial land within the municipality. Rochester has 35 hectares and Kyabram 98 hectares of industrial land.

The bulk of industrial land in the municipality is zoned Industrial 1 (408 hectares) or 86% of total industrial land. There are 65 hectares of land zoned Industrial 2 in Tongala and a small parcel of Industrial 3 land in Rochester.

There are five sites identified as future (unzoned) industrial land to the south of Echuca (173 hectares) and three sites in Tongala (118 hectares).

Industrial Activity

From July 2006 to December 2012 there were a total of 73 zoned industrial land subdivisions, with the large percentage (64%) located in Echuca, 25% in Rochester and 11% in Kyabram.

The majority (77%) of subdivisions resulted in industrial allotments sized less than 0.5 hectares. Of the 73 recently constructed industrial lots, 35 (48%) remain vacant as at December 2012.

From July 2006 to March 2012 there was an estimated total value of industrial building approval activity of approximately \$19.4 million, or an average of \$3.2 million per annum. Of this estimated construction value, 60% was for factory construction, the residual for warehouse construction.

Industrial Land Demand

From 2006 to 2012 on an average annual basis, 3.9 hectares per annum of industrial land has been consumed. The level of consumption by township includes:

- 1.6 hectares per annum – Tongala;
- 1.4 hectares per annum – Echuca;
- 0.7 hectares per annum – Rochester; and
- 0.2 hectares per annum – Kyabram.

Industrial Land Supply

In total, there is in excess of 15 years industrial zoned land across the municipality based on the average annual rate of land consumption in the period 2006 to 2012. In terms of future (unzoned) industrial land stocks it is estimated that there is approximately an additional 15+ years of supply.

The Report notes while there is adequate zoned land based on current consumption rates, consumption of industrial land should continue to be monitored to ensure there are sufficient land stocks to meet future demand

4. Community Consultation

4.1 External Authorities

A number of external state government agencies have been consulted regarding the preparation of this plan. These agencies included:

- Goulburn Broken Catchment Management Authority
- Department of Transport, Planning and Local Infrastructure
- Vic Roads
- Vic Track
- Coliban Water
- Various service authorities

As shown in Figure 6, reticulated water is available to the low density residential lots on the north west of the Precinct and Investigation Area. Coliban Water has advised that if this infrastructure was to be extended to service future developments, costs would be borne by the developer.

Reticulated sewer is not available to the Precinct or Investigation Area, therefore if this infrastructure was to be extended to service future development, costs would be borne by the developer.

Coliban Water has advised that, in the long term, the existing sewerage system is unlikely to have the capacity to service new development to the south of the existing urban area and a separate system will be required if development is to occur in this area.

The Goulburn Broken Catchment Management Authority (GBCMA) advised any further development of the Precinct would need to have consideration for flooding and the existing flood ways which are located throughout the Precinct. Of particular concern is the flow path located alongside Lady August Road, as discussed in Section 2.6 of this Report. It is essential the integrity of this floodway be maintained. The GBCMA was comfortable with the suggestion to include the Precinct in a Development Plan Overlay, which could include a requirement for flooding to be assessed and considered as part of any overall development proposal.

Consultation with the Department of Transport, Planning and Local Infrastructure (DTPLI) has revealed the need to protect the newly re-opened Echuca Toolamba Railway Line as it forms a critical part of the Victorian Freight Network. Development around the railway line should be of a lower density to ensure the amenity of future residents while allowing for the expected growth of the rail line.

4.2 Internal Council Departments

Internal council departments consulted in relation to specific issues include:

- Engineering – roads and infrastructure
- Statutory and Strategic Planning
- Commercial Business Analyst (Saleyards)
- Commercial Operations Manager (Aerodrome)
- Waste and Environment Co ordinator

The Aerodrome has been identified as an important facility for the town. There is currently a master plan in place for the Echuca Aerodrome which will expire in 2025. It has been advised that any works within the Precinct would need to have consideration for the future needs of the aerodrome as outlined in the master plan.

The Waste and Environment Coordinator is the officer of Council responsible for the operation of the Transfer Station. The facility operates during business hours and has had some minor issues in the past in relation to noise.

The operation of the Saleyards is overseen by Council's Commercial Business Analyst. It has been advised that there are approximately 28 sales per year and the operation of the facility is currently under review. The Saleyards are able to operate during the day and early evening however a curfew exists between 9:00pm and 6:00am as a result of a previous VCAT order.

4.3 Landowners and Community

Individual Meetings

Individual consultation has taken place with interested landowners in relation to site specific issues. For privacy reasons details of these meetings are not recorded in this Report.

Public Workshop/Meeting

Consultation was held with the landowners and wider community on 7 November 2013 by way of drop in sessions during the day and an interactive landowner/community workshop and meeting in the evening. The drop in sessions allowed for site specific discussions as well as broader Precinct ideas. Approximately 60 people attended the evening workshop in which the community were invited to respond both verbally and in written form to four key questions.

The four questions asked and a brief summary of the responses and other comments provided at the meeting are below. The specific answers and comments are detailed in Appendix 2 attached to this Report.

Question 1 - What does it mean to live in the Precinct?

Summary of Discussion

Living in the Precinct means different things to different people. For some it is the people in the area that is what makes it special. It was raised by a significant number of people that the Precinct provides for 'country' living in a location that is close to urban facilities in Echuca. Larger lots characterise the area with room to move, no close neighbours with a peaceful, private amenity. The location of the Precinct and its proximity to town is considered highly beneficial to residents. It is the access to these amenities and the rural living aspect which people living within the Precinct value the most.

Question 2 - What are the biggest issues facing the Precinct?

Summary of Discussion

Based on the responses received at the community meeting, the issues facing the Precinct are varied. In terms of access and infrastructure, access on to the Murray Valley Highway has been identified as a substantial issue which will be required to be addressed. Internal traffic congestion has also been raised as an issue within the Precinct, particularly around Scott Road on race event days. Lack of subdivision opportunities was also raised by some people. There are also interface issues, particularly in regard to the industrial interfaces. Environmental concerns have also been raised as issues for the Precinct, particularly around flooding. One of the largest concerns raised in the discussion on this question was the lack of community infrastructure available within the Precinct. One facility lacking from the Precinct is the availability of parks and playgrounds. It was noted that substantial residential growth has occurred in the west of Echuca in recent years, and these developments are provided with parks and other residential amenities/infrastructure.

Question 3 - Where do you want to be as a collective in 10 years?

Summary of Discussion

Previous studies have identified the land in the western edge of the investigation area to be rezoned for light industrial purposes to provide an appropriate transition/buffer between the existing residential uses and the industrial precinct. Independent Planning Panels have not previously supported these changes. It has been raised that the Investigation Area should not be rezoned to facilitate industrial development of land. Local residents have envisaged a significant increase in residential use and development densities with increases in services such as gas, walking/cycling tracks and other public facilities. Other infrastructure such as curb and channeling should also be provided as well as better managed lots. Some people wished to see the land develop further into traditional residential neighbourhoods.

Question 4 - How will we get there?

Summary of Discussion

The development of the Precinct and Investigation Area should be done in collaboration with Council and the individual developers to ensure that development occurs in a manner that increases population while maintaining rural living feel. It was felt that works to improve dangerous highway access points and railway crossings will need to be undertaken by Council in conjunction with road and rail authorities regardless of future development. A reduction in the minimum subdivision lot size will ensure that residents on larger properties are able to develop their properties. With further development, there will be a greater need for the expansion of the supply of existing reticulated services such as water as well as the introduction of natural gas and sewerage to the area. Any further changes to the development and land use patterns of the area will need to ensure that native flora and fauna are protected and where possible, enhanced. Clear, transparent and extensive community consultation will be required to change the development and land use patterns within the Precinct.

5. Policy Context

5.1 Regional Growth Plan

Regional Growth Plans have been developed to provide a broad direction for land use and development throughout Victoria and provide key regional centres and cities with a more detailed planning framework.

The Campaspe Shire forms part of the Loddon Mallee North Region, which also includes the municipalities of Mildura Rural City, Swan Hill Rural City, Buloke Shire and Gannawarra Shire.

The plan provides future direction within the region for Economic Development, Environment and Heritage, Population and Infrastructure. The plan highlights Echuca's significance as one of the major growth areas in the region and also its importance to surrounding smaller towns in the region, such as Rochester, in terms of service provision. The plan identifies Echuca as a regional centre which will continue to offer higher order services into the future, however acknowledges that these services will also be accessed, in some instances in Bendigo and Shepparton, both of which are outside of the Loddon Mallee North Region.

Manufacturing, Health, Education, Tourism and Agriculture have been identified as important industries which need to be retained and maintained for the ongoing sustainability of the region. These industries will need to be retained as well as further work to attract new investment and opportunities in the area.

Being such a large area, there are a large number of environmental issues which need to be considered as part of the development of the region. These include, but are not limited to

- Native Vegetation
- Flora and Fauna
- Waterway Health
- Erosion
- Salinity
- Bushfire
- Flooding
- Cultural Heritage

The plan also includes a broad direction for the future development of Echuca for future in terms of housing and its further development as a regional centre. Specific goals for the future of the town as a Regional Centre are:

- Facilitate growth of Echuca through planned development while considering population change and settlement planning in Moama.
- Reinforce Echuca's role as a regional centre and important tourism destination.
- Support the role of other urban localities to provide services and support to surrounding rural communities.
- Support the role of rural settlements to act as commuter towns within the wider network of Campaspe community of interest settlements and for neighbouring regions, for example to Bendigo or Shepparton.
- Support walking and cycling in urban areas to enhance accessibility.
- Implement the Echuca Housing Strategy (2011), Echuca South East Industrial and Commercial Growth Corridor Land Strategy (2011) and the Port of Echuca Revitalisation Project (2010).

The direction of the growth of the town will incorporate the recommendations of previous studies undertaken by the Campaspe Shire Council, as well as other State government policies and documentation.

Infrastructure and the transport network will be critical in catering for the projected future growth and population change. Population growth is unlikely to place significant additional strain on transport infrastructure, however the increase in the use the transport network for freight through the growth of agricultural and manufacturing industry will facilitate the need for expansion in the future.

5.2 State Policy

In determining how land should be used and developed, Council must have regard to the State Planning Policy Framework (SPPF). The Framework contains specific policies in relation to urban development, rural development, the environment, heritage and built form, housing, transport, infrastructure and economic development to ensure the orderly and consistent planning of Victoria.

Clause 16.02-1 is concerned with rural residential development and includes a number of strategies to manage development in rural areas and to identify land suitable for rural living and rural residential development.

In addition to the SPPF, Planning Practice Note No. 37 (November 2013) provides a clear direction for rural residential use and development. Whilst both the SPPF and the Practice Note recognise the planning scheme should ensure reasonable opportunities are found for rural residential development, as part of providing for housing diversity and choice, they also recognise rural residential development can have environmental, social and economic costs that are significantly higher than those of standard residential development.

Of particular relevance to this Precinct is *"The local environment and landscape should have the capacity to absorb more intensive use and development without significant or irreversible harm to its values or to the new use and development"*.

The Practice Note requires four broad questions to be answered in sequence:

- Strategy: Does rural residential development align with the overall strategic planning of the municipality?
- Housing need: How much rural residential development is required to provide appropriate housing diversity and choice to meet housing needs?
- Location: Where should new rural residential development take place?
- Subdivision and design: Is the new rural residential development subdivided and designed in an attractive setting offering high amenity and efficient infrastructure?

The Practice Note includes detailed guidance on managing future rural residential development in terms of protection of natural resources, environmental areas and biodiversity, landscape and heritage values, provision of social and physical infrastructure, land use compatibility and servicing capacity.

5.3 Local Policy

The Local Planning Policy Framework contains a number of strategies and objectives which relate specifically to the Campaspe Shire. The Framework provides for specific land use and localised policies.

At Clause 21.04-1 the settlement direction for Echuca is provided. It states that Echuca is the largest centre in the Shire with a population of about 12,280, although its catchment area is estimated to include 50,000 people within a 70 kilometre radius. The city is an important commercial, industrial, community, recreational and transportation hub for northern Victoria and southern New South Wales.

The development opportunities and constraints of the town relevant to this Precinct are as follows:

- *Echuca has substantial residential growth potential to the west.*
- *Growth potential to the east is constrained by the flood plain and to the north by the Murray River.*
- *Land to the south and south-east is separated from the existing residential area of the town by the industrial area and to the south west by the airport.*
- *The supply of low-density residential land has been estimated to represent at least 10 years demand which should adequately provide for the town's needs at this stage. The supply of low density residential development must not exceed projected needs for a 10 year planning timeframe.*
- *Low density estates should be located on poor quality agricultural land especially to the north west of the town in the vicinity of Wharparilla Drive.*

- *There are significant development opportunities at the Echuca Aerodrome. The Echuca Aerodrome Master Plan 2010 is to be used to guide the future growth and development of this site and its surrounds.*

6. The Echuca South East Rural Living Precinct Structure Plan

6.1 Overview of Recommendations

The Structure Plan provides an overarching guide to the future development and management of changes to land use, infrastructure, the natural environment and built form by setting out preferred directions for development.

The Structure Plan provides greater certainty for development and housing opportunities, while including traffic networks, environmental and locality considerations and interfaces with adjoining land uses.

Following review of the aforementioned sections of this Report, the following recommendations are made for the Structure Plan. This Structure Plan is not a development blueprint but rather a framework that establishes the key principles by which change will be allowed to occur over time.

In presenting the recommendations regard has been given to the objectives of this Structure Plan, Planning Practice Note Nos. 37 and 42, and the Planning Policy Framework of the Campaspe Planning Scheme.

For ease of conveying the recommendations, the Precinct has been divided into five separate areas:

- Area A – Rural Living Zone land bounded by Mary Ann Road, Lady Augusta Road, Kelsh Road and the Echuca Toolamba Rail Line.
- Area B – Rural Living Zone land between the Murray Valley Highway and Benson Road, on both sides of Lady Augusta Road
- Area C – Rural Living Zone land between Benson Road and Kelsh Road, east of Lady Augusta Road
- Area D – Rural Living Zone land west of Mary Ann Road
- Area E – Farming Zone land west of Mary Ann Road.

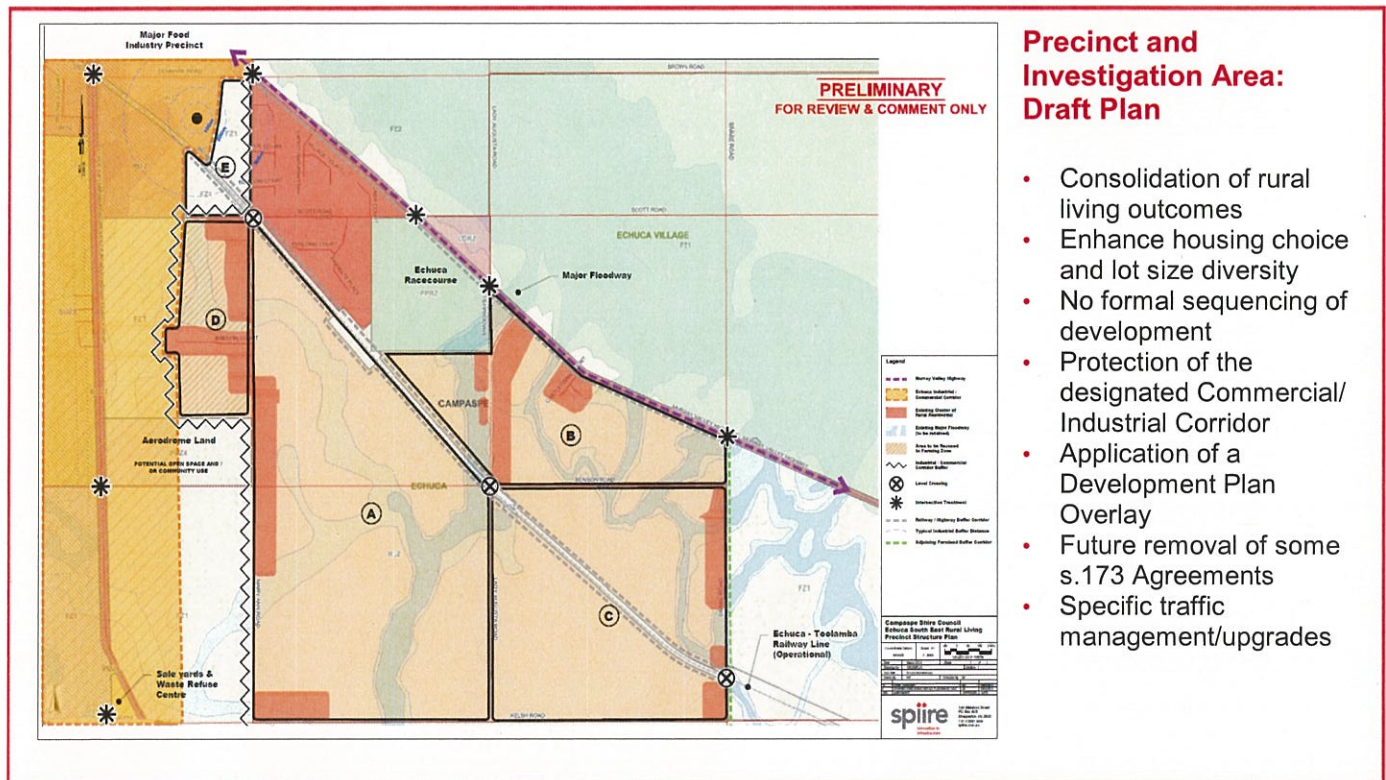


Figure 7 (Area Map)

6.2 Recommendations for the Whole Precinct

In addition to, and supportive of the recommendations specific to the above-mentioned areas, there is fundamental commentary relevant to the recommendations for the whole Precinct and Investigation Area.

Consolidation of the Rural Living Outcome

Having been identified and zoned for rural living purposes for some time, the development potential of this Precinct should be appropriately and responsibly maximised rather than Council needing to identify alternative areas near and around Echuca to be set aside, rezoned and developed for such purposes.

This rural living type of household choice can be offered in this Precinct, and through the implementation of this Structure Plan, an attractive, well planned and successful rural living neighbourhood will be created, providing housing supply for this market for many years to come.

Aside from being zoned for rural living outcomes, one of the key strengths to the Precinct is its proximity to the town centre of Echuca, the arterial road network, including Murray Valley Highway, and the growing employment corridor located to its west. It is by no means an isolated rural living site, and its development will not compromise or result in the loss of large-scale agricultural holdings.

In addition to the above, the Precinct is not suitable, nor will it be required to accommodate standard residential development. Campaspe Shire Council through the Echuca Housing Strategy has adopted the policy position of directing new standard residential growth to the west of Echuca, on land that is flood free, can be readily serviced and proximate to the Northern Highway.

Housing Choice and Lot Sizes

Provision of a wide variety and choice in housing types is becoming increasingly important, and is particularly needed to respond to the rapidly changing household structure and types. Clause 16.04-1 of the State Planning Policy Framework addresses housing diversity and includes the objective *“to provide for a range of housing types to meet increasingly diverse needs”*. A strategy to achieve this objective is *“to ensure housing stock matches changing demand by widening housing choice....”*,

Rural living is a recognised form of housing, and the creation of rural living allotments contributes not only to the supply of this type of lot, but also broadens the choice in housing types and styles available to the current and future residents of Echuca.

Rural residential lots are able to vary in size, generally upwards from 1 hectare. The ability for local municipalities to specify minimum subdivision sizes allows both a more site responsive outcome, as well as a variety of choice within the rural living range of lots, making it a suitable option for a wider variety of household types.

The Structure Plan recommends a series of minimum subdivision sizes to enable rural residential development to be designed in a manner which responds to the Precinct's attributes, including location of floodways and environmental features, buffers to/setbacks from adjoining land uses such as the Echuca Industrial and Commercial Corridor, farming land and agricultural activities, transport routes such as the Murray Valley Highway, the Echuca – Toolamba Railway Line and existing development.

Further, one of the main outcomes of the first workshop with the Precinct's landowners was an overwhelming desire of the residents to retain the rural character of the Precinct.

Sequencing of Development

Generally speaking, the sequencing of standard residential development should follow a logical progression, with each neighbourhood or stage rolling out as the adjacent one is completed or near completed. This allows for the efficient delivery of infrastructure and such services, and represents proper and ordered planning. However, given each of the above-mentioned areas has differing attributes and opportunities, and therefore will present to varying degrees a different lot size and housing choice to the market, a recommendation for the Echuca South East Rural Living Precinct Structure Plan is that,

the sequencing of development is not necessary. This is recommended for the following reasons:

- rural living lots are not dependent on the infrastructure services required for standard residential or even low density residential lots
- sequencing can place control of the land market into the hands of those who directly benefit (landowner/developer). This has potential to create a situation whereby a landowner may 'hold' land, thereby artificially creating a shortage and inflating the price of land
- landowners identified for short term development, may, for many various reasons, not wish to develop their land, unnecessarily restricting supply and causing frustration to those landowners further along the staging chain who may want to develop.

A similar approach was taken by the Independent Panel reviewing Amendment C46 to the South Gippsland Planning Scheme, which sought to introduce the Foster Structure Plan. In its Report the Panel stated:

"As far as sequencing of land releases is concerned, the Panel observes that rural living lots are not dependent on the infrastructure services required for Residential 1 Zone and Low Density Residential Zone lots. Therefore, the Panel agrees with submitters that it is not necessary to sequence investigations and/or release of land for rural living as recommended in the Foster Structure Plan. The Panel finds no particular justification for a staging plan for rural living subdivision".

Removal of s173 Agreements

As stated previously, there are several large "balance lots" in the Precinct and Investigation Area which are subject to s173 Agreements, the provisions of which prevent further subdivision. Some of these include a clause indicating the agreement may end if the land is rezoned or more intensive subdivision of that land is permitted.

Many of these balance lots make little meaningful contribution to either residential or agricultural land uses. In light of this Structure Plan and recommendations, there is an opportunity for these lots to have their restrictions removed to allow a more meaningful contribution to the development and character of the Precinct.

Given the changes to the minimum subdivision size recommended in this Structure Plan, and the desire to better utilise the "balance lots", it is recommended at the time a Development Plan is submitted to the Council for approval, if relevant, it include application for the ending and therefore removal of the agreement from the title to the property.

6.3 Recommendations for Individual Areas

6.3.1 Area A – Land Between Mary Ann Road and Lady Augusta Road.

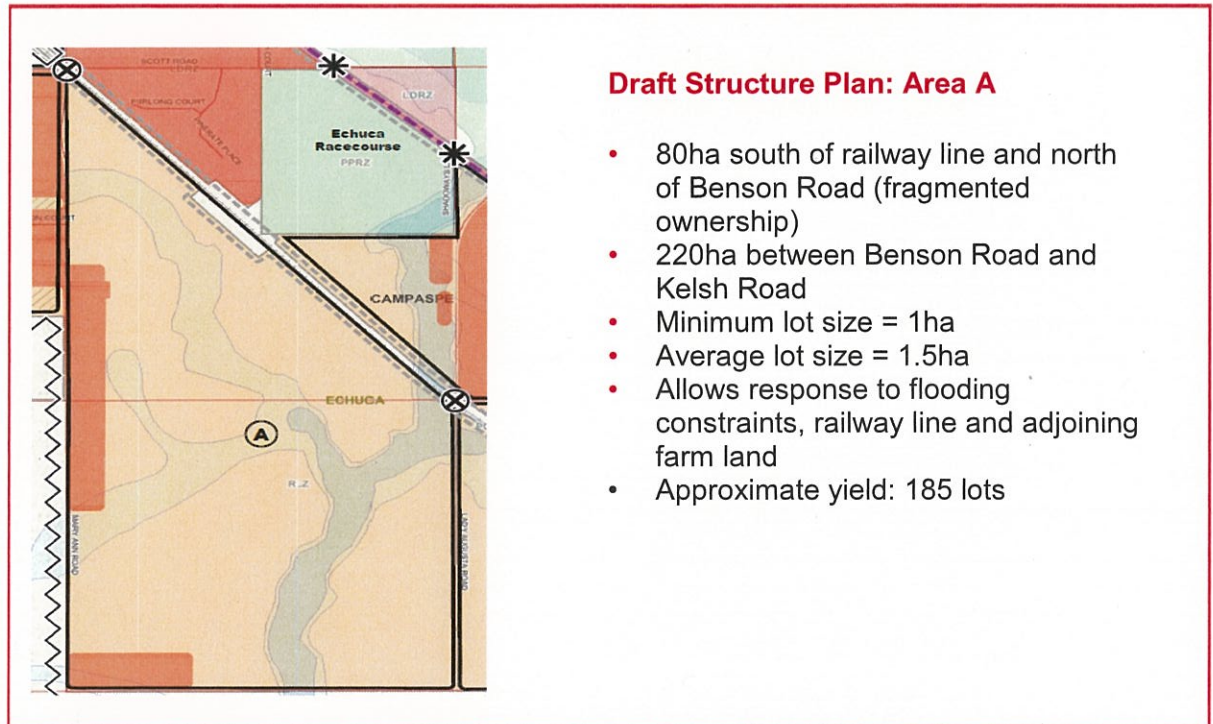


Figure 8: Area A Draft Structure Plan

Summary of features:

- Rural Living Zone
- Bound by Mary Ann Road to the west, Kelsh Road to the south, the railway line to the north and Lady Augusta Road to the east.
- The Echuca – Toolamba Railway Line is the northern boundary of the Area.
- The land comprises a variety of lots sizes along Mary Ann Road, most of which are established with rural residential uses. Some larger lots extend east to the Echuca – Toolamba Railway Line.
- There is approximately 82 hectares of undeveloped land south of the railway line.
- The railway crossing at Lady Augusta Drive is likely to require further investigation into the need for works to improve safety for both vehicles and trains.
- South of Benson Road there are thirteen lots fronting Mary Ann Road. These are developed for rural residential purposes.
- A further nine lots are also similarly developed and front to both Kelsh and Mary Ann Roads.
- There is balance area of approximately 220 hectares which is currently farmed.

- A major flow path traverses the centre and east the area south of Benson Road, exiting near the intersection between Lady Augusta and Benson Roads.
- Land to the west and south is within the Farming Zone and is actively farmed.
- Land to the east, across Lady Augusta Road is within the Rural Living Zone, but is also actively farmed.

Relevant History

- As a result of the previous subdivision, a balance area of approximately 110 hectares to the north of Area A is subject to a s173 Agreement preventing any further subdivision.

Structure Plan Recommendations for Area A

- For Area A, the Structure Plan recommends a minimum lot size of 1 hectare and an average size of 1.5 hectares, enabling a lot layout to respond to features of the area including the existing pattern of development, particularly along Mary Ann Road and Lady Augusta Road south of the railway, the major floodway along Lady Augusta Road, the Echuca Racecourse and to incorporate any setbacks required from the Echuca – Toolamba Railway line whilst still providing well placed and well sized building envelopes. Larger lots should be located around the periphery of the Area and along the rail line to reduce the potential for future land use conflict in these areas.
- Providing for development which has lots subdivided at an average size 1.5 hectares will provide a more rural character, whilst still recognising the proximity of the Area to nearby low density residential development, the Murray Valley Highway and Echuca town centre.

6.3.2 Area B – Land Bound by Murray Valley Highway and Benson Road, east of Lady Augusta Road

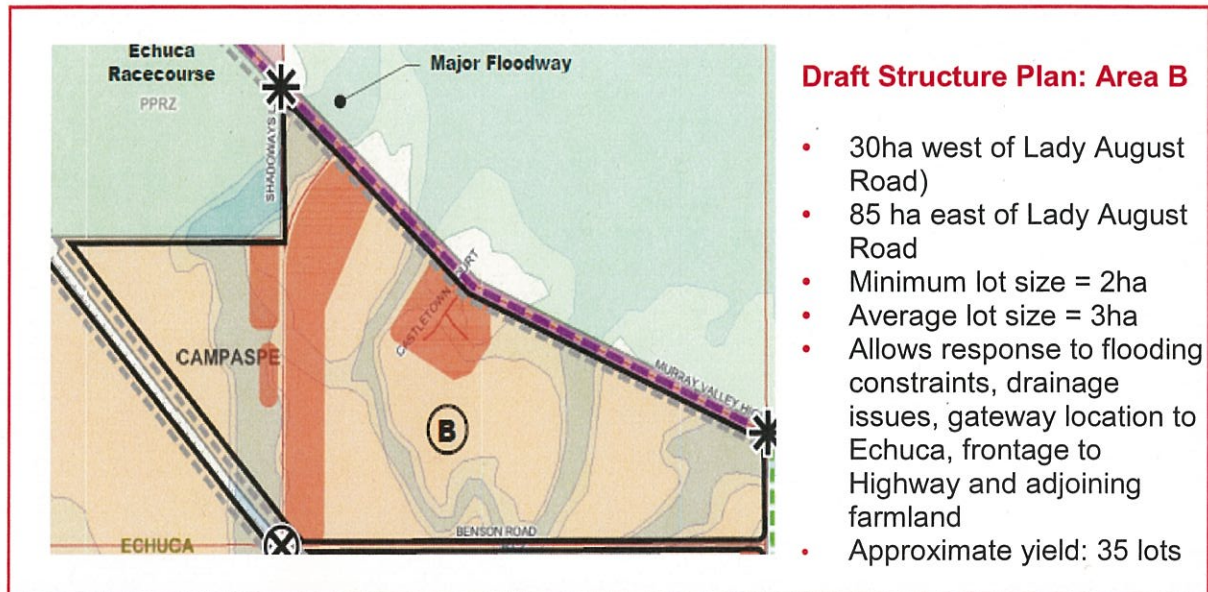


Figure 9: Area B Draft Structure Plan

Summary of features:

- Rural Living Zone
- Bound by the Murray Valley Highway to the north, a major arterial road for northern Victoria.
- Also bound by the Echuca Racecourse and rail line to the west, Benson Road to the south, and Simmie Road to the east.
- The area north of the railway line is particularly prone to flooding, with a major flow path traversing the area within proximity to Lady Augusta Road.
- A balance area of approximately 30 hectares is located to the western side of Lady Augusta Road.
- There are two clusters of rural residential development, one comprising 16 lots fronting to Lady Augusta Road and a further lots around Castletown Court, which is accessed from the Highway. There is no connectivity between these areas.
- The area has in part previously been subdivided and includes 5 lots, each of approximately 8,000m² which front to Lady Augusta Drive. These lots are occupied by established dwellings, gardens and shedding.
- Amendment C34 proposed to amend the Schedule to the Rural Living Zone to allow the land on the western side of Lady Augusta Road to be subdivided into lots with a minimum size of 0.5 hectares and an average lots size of 1.0 hectare. The amendment also proposed to apply a Development Plan Overlay to the land to restrict the development of lots to uses associated with the Echuca Racecourse. Following their consideration of the proposal, the Panel concluded the amendment should be abandoned. It was and the land remains undeveloped, subject to the s173 Agreement.

- The area is quite flood impacted.
- There is balance area of approximately 85 hectares which is currently farmed.
- Land to the east is within the Farming Zone and is actively farmed.
- Land to the south, across Benson Road is within the Rural Living Zone, but is also actively farmed.
- The intersection of the Murray Valley Highway and Simmie Road signals an entry point into Echuca from the surrounding agricultural and rural areas.

Relevant History

- As a result of the previous subdivision of the area north of the railway line and south of the racecourse, the balance area is subject to s173 Agreement preventing any further subdivision.
- As a result of the previous subdivision, a balance area of approximately 85 hectares is subject to a s173 Agreement in regard to drainage. The agreement does not prevent the further subdivision of the land.

Structure Plan Recommendations for Area B

- For Area B, the Structure Plan recommends a minimum lot size of 2 hectares and an average size of 3 hectares. These slightly larger lots will enable an appropriate design response to the area's more significant flood prone areas, the drainage issues identified in the current s173 Agreement, the area's frontage to the Murray Valley Highway at a 'gateway' location to Echuca, where farming/rural landscapes transitions towards more urban and built up landscapes, and the farming land to the east.

6.3.3 Area C – Land Bound by Benson and Kelsh Roads, east of Lady Augusta Road

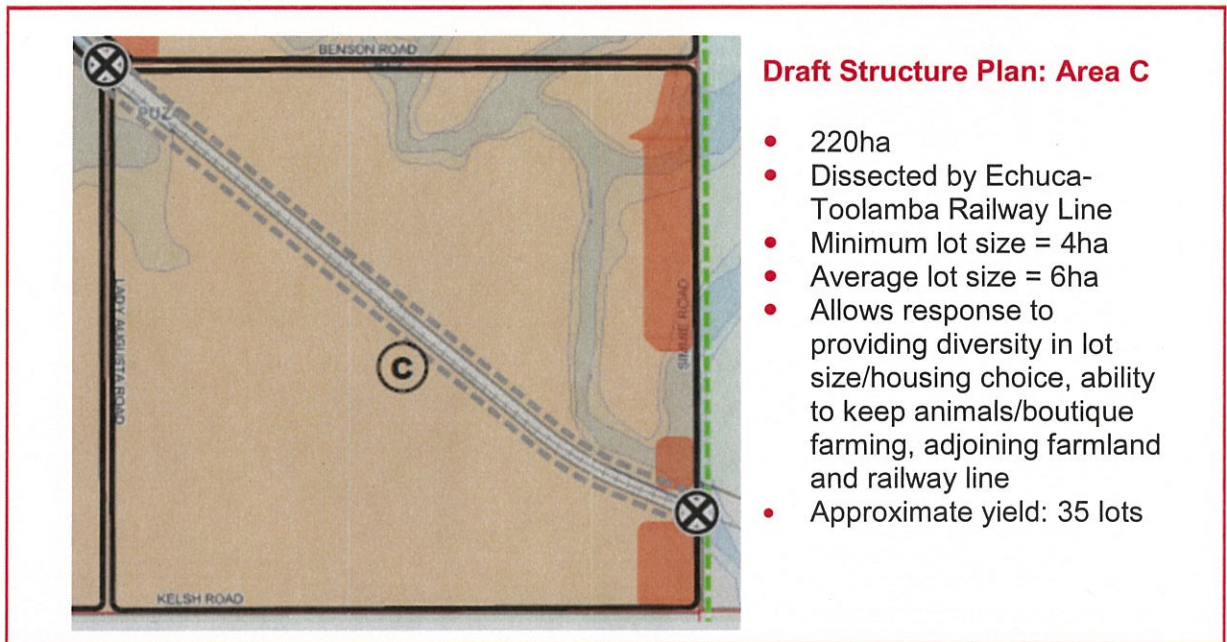


Figure 10: Area C Draft Structure Plan

Summary of features:

- Rural Living Zone
- Bound by Lady Augusta Road to the west, Benson Road to the north, Simmie Road to the east and Kelsh Road to the south.
- For the majority of the length of Simmie Road there are rural residential lots, the majority of which are developed with dwellings, gardens and shedding.
- The Echuca – Toolamba Railway Line dissects the Area.
- There is balance area of approximately 220 hectares which is currently farmed.
- Land to the east is within the Farming Zone and is actively farmed.
- Land to the south, across Benson Road is within the Rural Living Zone, but is also actively farmed.

Relevant History

- As a result of the previous subdivision, the majority of the balance area is subject to s173 Agreements preventing the further subdivision of land.

Structure Plan Recommendations for Area C

- For Area C, the Structure Plan recommends a minimum lot size of 4 hectares and an average size of 6 hectares. These larger rural living lots will contribute to the choice in rural living opportunities as there is likely to be a demand for such larger lots in this Precinct, particularly for the keeping of horses, a small number of stock

and/or for boutique or niche agricultural pursuits. This area is also interfaced with agricultural land to its east and south, land which is zoned and activity used for farming, and which forms part of the broader farming area south east of Echuca. Larger lots are appropriate along this interface to both respect the adjoining agricultural activities, and to minimise potential conflict between these activities and the new dwellings.

- The larger lot sizes also enable to any setbacks requirements from the Echuca – Toolamba Railway line to be incorporated into the lots whilst still providing well placed and well sized building envelopes.

6.3.4 Area D – Rural Living Land West of Mary Ann Road

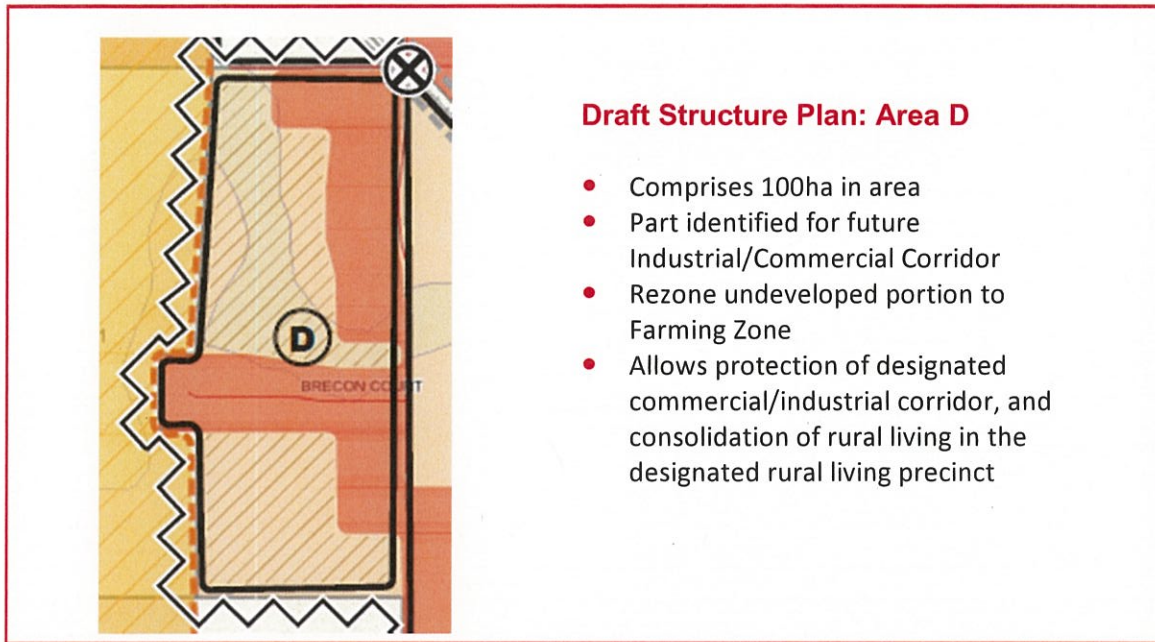


Figure 11: Area D Draft Structure Plan

Summary of features:

- Rural Living Zone
- One lot sits partially within the area extends into the land identified in the Industrial Study. This lot is within the Farming Zone and also forms the western boundary of the area.
- Bound by Mary Ann Road to the east, Echuca Aerodrome land to the south, Scott Road to the north and Farming Zone in the same lot and the Echuca Aerodrome is to the west.
- The area has been previously subdivided with approximately 16 lots fronting to Mary Ann Road, 3 to Scott Road and 20 lots along both side of Brecon Court, which is accessed from Mary Ann Road. These lots range in area from 4,000m² to 8,000m² and the majority are occupied by established dwellings, gardens and shedding.
- A balance area of approximately 100 hectares is located to the rear of the rural residential lots. Part of this area is within the Rural Living Zone but the majority is in the Farming Zone.
- Although not yet zoned, much of the balance land forms part of the designated Echuca South East Commercial and Industrial Corridor.
- Land opposite Area A is subdivided into a variety of allotment sizes, but the majority are used for rural residential purposes.

Relevant History

- As a result of the previous subdivisions, the balance area is subject to a s173 Agreement preventing any further subdivision.

Structure Plan Recommendation

The Structure Plan recommends rezoning the balance lot, subject to the S.173 agreement preventing further subdivision, to Farming Zone for the following reasons:

- Council has committed to designating and advancing the Echuca South East Industrial/Commercial Corridor, and the decision to retain a buffer to sensitive uses reinforces the integrity of the Corridor. The buffer provides for a threshold distance of at least 200 metres, making the industrial land suitable for a range of uses, including food and beverage manufacturing.
- All land subject to the s173 Agreement is being actively farmed for cropping, therefore the Farming Zone is an appropriate planning tool available in the Victorian Planning Provisions to apply to this area. Amendment C86 & the Industrial Strategy recognised in the new structure maps for Echuca that development should not occur on these parcels
- As the Industrial/Commercial Corridor develops, some of the land currently zoned Farming will likely be rezoned to enable it to fulfil its designated purposes. Planning Practice Note No. 42 Applying the Rural Zone (November 2013) includes as possible Farming Zone areas, those areas where non-farming uses and development need to be strictly controlled so that potential land use conflicts can be avoided. As such, the application of the Farming Zone will remain appropriate as the adjoining land is developed for industrial and/or commercial purposes.
- The application of the Farming Zone does not create an expectation of sensitive land uses, other than perhaps one dwelling per property to support an agricultural pursuit, thereby minimising potential conflict sources to future industrial development in the adjoining Industrial/Commercial Corridor.
- Minimising potential conflict sources enhances the attractiveness of the Corridor and provides heightened confidence for long term investment in the Corridor.
- In assessing Area D and its location proximate to the designated Corridor, regard has been given to the EPA's "Recommended separation distances for industrial residual air emissions" (March 2013) and the provisions of Clause 52.10 of the Campaspe Planning Scheme "Uses with potential adverse amenity impacts".
- Planning Practice Note No. 37 Rural Residential Development (November 2013) states:

"The nature of existing and possible uses of land adjacent to the land proposed to be rezoned must be documented in an assessment accompanying the proposal report. A map showing site context must be provided.

The assessment must also include consideration of the potential impacts of the proposal on adjoining and nearby land.

Rural residential development should not be provided on land that is:

- *within the separation requirements of an amenity reducing land use as described in the Environment Protection Authority's Recommended Separation Distances for Industrial Residual Air Emissions, and is a 'sensitive' land use in applying the EPA guidelines*
- *adversely affected by noise levels generated by adjoining land uses, including wind energy facilities*
- *within the separation distance prescribed in the Industrial 1 Zone, Industrial 2 Zone and Industrial 3 Zone between industrial activity and a sensitive use.*
- Area D is provided with irrigation water via a channel located between Area A and the adjoining industrial land. This infrastructure provides opportunities for boutique or niche agricultural activities.
- This area is also affected by the Land Subject to Inundation and Floodway Overlays. Council is planning to construct a retardation basin on the western edge of this land to address the drainage problems in the area.

6.3.5 Area E – Farming Zone Land West of Mary Ann Road

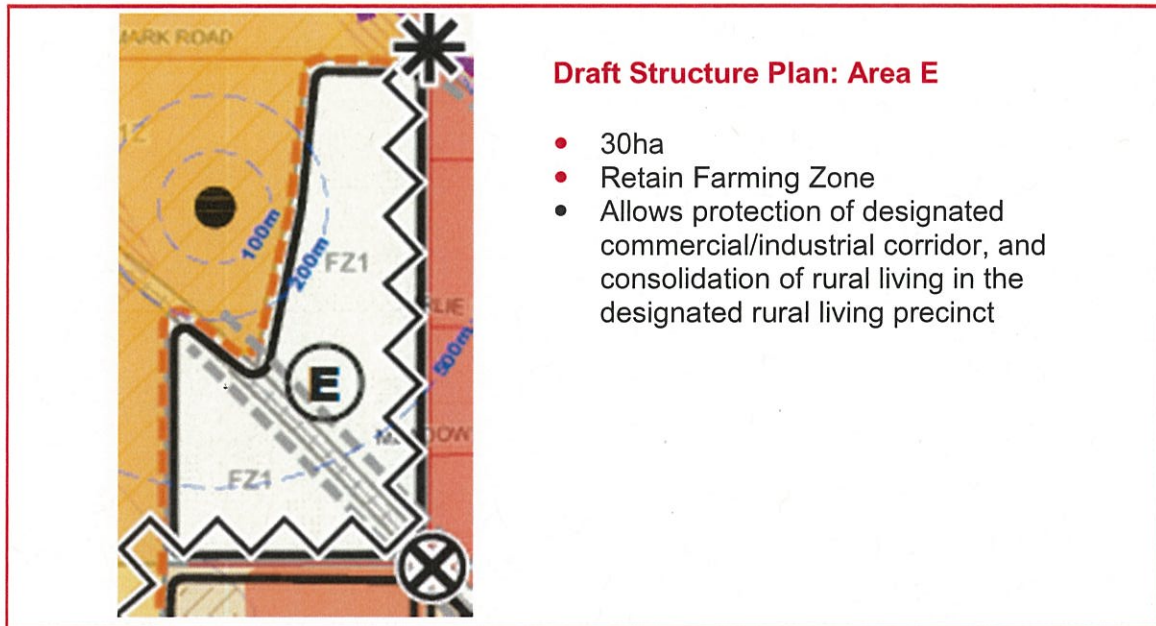


Figure 12: Area E Draft Structure Plan

Summary of features:

- Farming Zone
- Comprises approximately 30 hectares contained in five titles, four being north of the railway line
- Bound by Mary Ann Road and low density residential development to the east, Scott Road to the south, Denmark Road to the north and Industrial 1 Zone land to the west.
- The area is dissected into two portions by the Echuca – Toolamba Railway Line.
- The low density residential development to the east comprises lots of approximately 4,000m², the majority of which are improved with established dwellings, gardens and shedding.
- The industrial land to the immediate west is vacant but forms part of the recently approved Echuca South East Commercial and Industrial Corridor. It is owned by Council and is identified by lots of approximately 2,500m². Importantly, the area to its north, on the opposite side of Denmark Road, is a major food industries precinct with established businesses currently operating.

Relevant History

- The RPD Review of Rural Living recommended the buffer areas between the industrial precinct (west) and residential land (east) be retained as Rural (now Farming) or at the most rezone to Rural Living, but prevent further subdivision, and ensure appropriate dwelling setbacks are maintained. The Farming Zone remains in place.

- The Echuca South East Commercial and Industrial Corridor Strategy recommended the area be retained as a buffer, and included in the Industrial 3 Zone. The Panel considering Amendment C86 did not support the inclusion of this area in the Industrial 3 Zone, and recommended further investigation into this area and the wider Precinct.

Structure Plan Recommendation

The Structure Plan recommends retaining Area E in the Farming Zone for the following reasons:

- Council has committed to designating and advancing the Echuca South East Industrial/Commercial Corridor, and the decision to retain a buffer to sensitive uses reinforces the integrity of the Corridor. The buffer provides for a threshold distance of at least 200 metres, making the industrial land suitable for a range of uses, including food and beverage manufacturing.
- Whilst the land is not actively farmed in the traditional sense, the Farming Zone is an appropriate planning tool available in the Victorian Planning Provisions to apply to this area.
- Planning Practice Note No. 42 Applying the Rural Zone (November 2013) includes as possible Farming Zone areas, those areas where non-farming uses and development need to be strictly controlled so that potential land use conflicts can be avoided.
- The application of the Farming Zone does not create an expectation of sensitive land uses, other than perhaps one dwelling per property to support an agricultural pursuit, thereby minimising potential conflict sources to future industrial development in the adjoining Industrial/Commercial Corridor.
- Minimising potential conflict sources enhances the attractiveness of the Corridor and provides heightened confidence for long term investment in the Corridor.
- In assessing Area E and its location proximate to the designated Corridor, regard has been given to the EPA's "Recommended separation distances for industrial residual air emissions" (March 2013) and the provisions of Clause 52.10 of the Campaspe Planning Scheme "Uses with potential adverse amenity impacts".
- Planning Practice Note No. 37 Rural Residential Development (November 2013) states:

"The nature of existing and possible uses of land adjacent to the land proposed to be rezoned must be documented in an assessment accompanying the proposal report. A map showing site context must be provided.

The assessment must also include consideration of the potential impacts of the proposal on adjoining and nearby land.

Rural residential development should not be provided on land that is:

- *within the separation requirements of an amenity reducing land use as described in the Environment Protection Authority's Recommended Separation Distances for Industrial Residual Air Emissions, and is a 'sensitive' land use in applying the EPA guidelines*

- *adversely affected by noise levels generated by adjoining land uses, including wind energy facilities*
- *within the separation distance prescribed in the Industrial 1 Zone, Industrial 2 Zone and Industrial 3 Zone between industrial activity and a sensitive use.*
- Area E is provided with irrigation water via a channel located between Area A and the adjoining industrial land. Similar to other farming zoned areas within the surrounding area, Area E is serviced with an irrigation channel which is located between Area E and the industrial land. This infrastructure provides opportunities for boutique or niche agricultural activities.
- The Farming Zone permits a number of uses that could potentially suit Area E and in doing so retain the rural character of the Precinct, including niche or boutique agricultural activities, tourism attractions such as The Great Aussie Beer Shed which is further along Mary Ann Road, home occupations and garden nurseries. The properties in Area E are provided with irrigation water via a channel located between Area E and the adjoining industrial land.
- In the context of Area E, other zone options were given consideration, and deemed inappropriate as follows:
 - General Residential Growth Zone – the area does not form part of Echuca's established or future standard residential growth corridor
 - Low Density Residential Zone – this would create an expectation for more sensitive land uses and subdivision, which is not appropriate given the Area's proximity to a major industrial/commercial corridor
 - Rural Living – similar to the application of the Low Density Residential Zone. Even if this Zone was applied and the minimum subdivision size set to prevent further subdivision of Area E, the provisions permits a number of sensitive land uses and developments which could compromise the integrity of the industrial/commercial corridor.
 - Industrial 3 Zone – already considered and dismissed by the Independent Panel for Amendment C86, mainly due to potential adverse impacts on the rural residential allotments east of Mary Ann Road.
 - Rural Activity Zone – this zone is applied to rural/non-urban areas of the municipality where there is a policy direction to provide for tourism and recreational activities, including alongside the environs of the Murray River.
 - Rural Conservation Zone – the area does not hold any significant conservation value, nor is to located within a rural context or in an area of biological or ecological significance
 - Green Wedge Zone – this zone is generally applied to areas where tourism or recreation can support surrounding farmland, or where features of the land is important, including, for example, native vegetation, flora and fauna, cultural heritage, significant habitats, or landscape and visual qualities of the land.

6.4 Recommendations for the Investigation Area

Investigation has been undertaken into the remaining investigation areas to the north east and south of the Precinct. At this time it is not considered changes are necessary in these areas.

6.5 Implementation of the Recommendations

Application of a Development Plan Overlay

A Development Plan Overlay is an effective way for Council's to ensure the area is analysed and understood, and the key features and attributes are responded to appropriately. The Development Plan Overlay gives greater certainty to development, and provides a framework against which application for planning approval for subdivision can be assessed.

The purposes of the Development Plan Overlay are:

- To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To identify areas which require the form and conditions of future use and development to be shown on a development plan before a permit can be granted to use or develop the land.
- To exempt an application from notice and review if it is generally in accordance with a development plan.

It is recommended that the Precinct be included in a Development Plan Overlay. The schedule to the Development Plan Overlay should include:

- a requirement for a Precinct wide investigation into flooding and drainage, and traffic management to be undertaken prior to any development plans being prepared for the individual areas.
- a requirement for separate development plans to be approved in Areas A, B and C. (Subject to further analysis at the time this Structure Plan is being implemented, it may be appropriate to separate Area A into A1 (north of Benson Road) and A2 (south of Benson Road), especially given the differences in subdivision patterns between these two sub-areas and greater number of landowners in Area A1).

It is also recommended Campaspe Shire Council co-ordinate these investigations into flooding and drainage, and traffic management for the future development of the Precinct.

Subject to further refinement and input from relevant authorities, it is suggested a schedule to the Development Plan Overlay require the development plans for each Area to consider, assess and respond to:

- How the proposed subdivision layout and development of the land including roads, lot boundaries streetscape treatments, and landscaping will integrate with the surrounding area
- Diversity in allotment sizes and dimensions to encourage variety in dwelling types.
- The stages, if any, of the subdivision and a timetable of any staging of future development of that land.
- Pedestrian and bicycle network connections, including links to any networks on abutting land and access to public transport services.
- The location of any significant environmental, cultural, heritage and/or ecological (faunal and/or floral) features including remnant vegetation.
- Drainage management, including the location of any on-site drainage retention facilities.
- How the proposed development addresses any flood impacts.
- Arrangements for the provision of all appropriate infrastructure and services.
- The provision of buffers from land used for agricultural purposes, from the Echuca – Toolamba Railway Line and other sensitive land uses.
- The preservation of any significant trees or vegetation identified in the flora and fauna assessment.

As appropriate, it is further recommended a development plan be accompanied by:

- An environmental assessment of the land, involving a flora and fauna survey, which identifies the health and habitat value of all native vegetation.
- A survey of the area for aboriginal archaeological sites.
- A stormwater management plan detailing how stormwater will be collected and treated within the development, and how that process will not impact adversely on the natural flood carrying capacity of the local watercourses.
- Any drainage management plan to ensure that no increase in flood impacts of properties that were not the subject of the development plan under consideration.
- A preliminary soil assessment demonstrating the extent of any contaminated soils that may exist on the subject land, and if detected, a more detailed assessment outlining the location of the contaminated soil, the type of contaminants detected, and the strategies required being undertaken to de-contaminate the affected areas.
- A Traffic Management and Impact Mitigation Plan that includes the identification of appropriate access and circulation of vehicles on the existing and future road network, and upgrade works necessary to accommodate traffic generated by the development and to mitigate the impact of the development.
- A Land Capability Assessment carried out by a suitably qualified consultant to ensure the future lots are large enough and suitable for disposal of waste and effluent. This is particularly relevant where the flood overlays exist across parcels of land.

Application of a Development Contributions Plan Overlay

A Development Contributions Plan Overlay is used to identify areas which require the preparation of a development contributions plan for the purpose of levying contributions for the provision of works, services and facilities before development can commence.

A likely outcome of the investigations into flooding and drainage, and traffic management across the Precinct is the identification of new and/or improved items of infrastructure needed to service or cater for the future development of the Precinct. The Development Contributions Plan Overlay is the most appropriate planning tool Council can use to equitably apportion the costs of these works, services and facilities across the Precinct, the Development. The requirement to make the contribution is generally triggered at the time of development/construction.

As stated in the Overlay, the development contributions plan must:

- Specify the area to which the plan applies.
- Set out the works, services and facilities to be funded through the plan, including the staging of the provision of those works, services and facilities.
- Relate the need for the works, services or facilities to the proposed development of land in the area.
- Specify the estimated costs of each of the works, services and facilities.
- Specify the proportion of the total estimated costs of the works, services and facilities which is to be funded by a development infrastructure levy or community infrastructure levy or both.
- Specify the land in the area and the types of development in respect of which a levy is payable and the method for determining the levy payable in respect of any development of land.
- Provide for the procedures for the collection of a development infrastructure levy in respect to any development for which a permit is not required.

It is also recommended Campaspe Shire Council co-ordinate the preparation of the Development Contributions Plan, and subsequent planning scheme amendment, and seeks to commence this work upon completion of the necessary investigations which will inform this Contributions Plan.

Note: As recently announced by the Minister for Planning, from 1 July 2015 new Standard Levies for development contributions in priority growth locations will be available for use, as will the new Infrastructure Contribution Plan. Depending on the timing of the implementation of this Structure Plan, these new planning tools will be worthy of consideration implementing the above recommendation.

Amendment of the Schedule to the Rural Living Zone

As outlined in Section 6.3 above, the Precinct has been divided into spate Areas, for which different minimum subdivision sizes have been recommended. In order to give effect to these recommended lot sizes, the schedule to the Rural Living Zone as it currently exists in the Campaspe Planning Scheme will need to be amended. In summary, the Schedule will need to be amended to allow:

- Area A – minimum lot size of 1 hectare, with an average size of 1.5 hectares
- Area B – minimum lot size of 2 hectares, with an average size of 3 hectares
- Area C – minimum lot size of 4 hectares, with an average size of 6 hectares

Summary of Implementation Process, and Subsequent Development Process

The following implementation process is based on the assumption the Structure Plan has been adopted by the Council.

Following adoption of the Structure Plan:

- As a priority, Council to co-ordinate the Precinct wide investigations into flooding and drainage and traffic management, and the development contributions plan for the Precinct.
- Preparation and exhibition of a planning scheme amendment to:
 - provide support for the development of the Precinct in the Local Planning Policy Framework,
 - include the Precinct in a Development Plan Overlay,
 - include the Precinct in a Development Contributions Plan Overlay, and to
 - amend the Schedule to the Rural Living Zone to allow a reduced minimum subdivision size for the individual Areas of the Precinct
- This amendment may be determined by an Independent Planning Panel appointed by the Minister for Planning.
- If the amendment is supported and the Campaspe Planning Scheme amended accordingly, individual landowners can then prepare and submit to Council development plans for the Area within which their property is located.
- Once the development plan has been completed to the satisfaction of the Council, the landowner/developer is then able to submit to the Council a planning permit application for the development/subdivision of land. In some circumstances, Council may accept a combined request to approve a development plan and planning permit application. Conditions of approval will include the requirement for contributions to be made as per the provisions of the Development Contributions Plan Overlay.

6.6 Traffic Recommendations – Precinct and Investigation Area

- Road Conditions

As the Precinct develops, greater volumes of traffic will be utilising the existing road network. It is recommended that all roads in the Precinct be classified as Rural Living Access Roads and that all roads are progressively sealed to a width of not less than 6.2m as dictated by demand. This would be undertaken at the time of development, at the expense of developers, through consultation with Council.

- Intersection Treatments

The intersection treatments identified are summarised below.

Murray Valley Highway

- Channelised right turn lanes are justified at each of the highway intersections with Denmark Road, Scott Road, Lady Augusta Road and Simmie Road. An example of a channelised right turn lane is shown in Figure 13.
- Additional lanes for left turn movements at these intersections are not required to cater for traffic moving to and from the Precinct. However a left turn lane at Denmark Road to cater for traffic travelling between the Murray Valley Highway's southeast approach and industrial businesses along Denmark Road and west of Echuca-Kyabram Road is anticipated to be appropriate. An example of a left turn lane is shown in Figure 14.



Figure 13: Aerial photo of a typical channelised right turn lane (Type CHR treatment) (Courtesy of Trafficworks license to NearMap)



Figure 14: Aerial photo of a typical left turn lane treatment (Type AUL(S) treatment)(Courtesy of Trafficworks license to NearMap)

Echuca-Kyabram Road

- Additional lanes for left turn movements at Echuca-Kyabram Road's intersections with Denmark Road, Benson Road and Kelsh Road are justified.
- Additional lanes for right turn movements at these intersections are not required to cater for traffic to and from the RLZ Precinct. Widening and sealing of the west side shoulders will provide suitable treatments. An example of a widened shoulder opposite a side road at is shown in Figure 15



Figure 15: Aerial photo of a typical basic right turn treatment, a widened shoulder, to assist right turn movements at low traffic volume intersections (Type BAR treatment) (Courtesy of Trafficworks license to NearMap)

It is recommended that the following intersection treatments be provided to improve safety and traffic flow for vehicles travelling to the Precinct.

- Along the Murray Valley Highway:
 - Channelised right turn lane treatments at the Denmark Road, Scott Road, Lady Augusta Road and Simmie Road intersections
- Along Echuca-Kyabram Road:
 - Left turn lane treatments at the Denmark Road, Benson Road and Kelsh Road intersections.
 - Widening and sealing of the west side shoulders past the Benson Road and Kelsh Road intersections.

Treatment of the Echuca-Kyabram Road intersection with Denmark Road is complicated by the Echuca-Toolamba rail level crossing. Whilst the Precinct will generate minimal, if any right turning traffic at this intersection improvements are required to ensure it operates satisfactorily and with a suitable level of safety in the future.

These improvements will improve traffic and safety for traffic turning right from Echuca-Kyabram Road such as:

- Trucks travelling from the south to access food processing businesses located along Denmark Road; and

- Stock trucks which chose to use Denmark Road as a link between the Murray Valley Highway and the Echuca Livestock Exchange located on Echuca-Kyabram Road south of Keels Road.

The appropriate treatment for this intersection is likely to incorporate either a roundabout or traffic signals with possible realignment of Denmark Road required. Determination of the appropriate treatment for this intersection requires detailed assessment beyond the scope of this Precinct Structure Plan.

It is recommended that detailed assessment be undertaken of the needs of the Echuca-Kyabram Road / Denmark Road intersection to determine appropriate improvements to ensure the intersection operates satisfactorily in the long term.

The Precinct does not currently have any specific pedestrian and bicycle facilities with almost all trips being undertaken by passenger vehicles. As the Precinct is developing it can be expected that it will be accompanied by a community desire for facilities to support recreation walking and cycling in the Precinct. Additionally the topography of the Precinct and the land around it is suitable for cycling to / from Echuca for many purposes such as travel to work, school, recreation and social activities and for recreational cycling.

It is appropriate that walking and cycling in the Precinct be supported by actions such as:

- Providing shared paths along:
 - Busier roads such as the grid network roads, e.g. Mary Ann Road, Lady Augusta Road, Simmie Road, Scott Road and Benson Road;
 - The Echuca-Toolamba rail line which can provide a path through the “middle” of the Precinct which efficiently links the grid roads and links to Echuca;
 - Echuca-Kyabram Road and possibly through the industrial area west of Echuca-Kyabram Road.
- Providing shared path links between courts that are expected to developed in the larger blocks bordered by the grid roads.
- Including shared paths within the open spaces that may developed in the areas that are flood prone.

It is recommended that walking and cycling plans be prepared to encourage non-vehicle movements within the Precinct and between the Precinct and Echuca by developers at the time of development plan approval.

- Public Transport - Road Based

School buses operate through Precinct. The number of services can be expected to increase as residential development occurs and more students reside in the Precinct. Additionally as development occurs the potential for public bus services within the Precinct will increase.

It is recommended that design of future road improvements should include consideration of bus service needs including location and access to bus stops and design of estates to comply with PTV accessibility criteria.

- Public Transport - Rail Transport

The Echuca-Toolamba Rail line is located within the Precinct. It is estimated that as development of the Precinct occurs there will be approximately 165 new residences south of the rail line. This will increase traffic flows over the rail crossings by approximately 1,600vpd.

It is estimated that the additional trips will be distributed to the existing level crossing as follow:

- Echuca-Kyabram Road – 800vpd.
- Mary Ann Road – 200vpd.
- Lady Augusta Road – 400vpd.
- Simmie Road – 200vpd.

The increased traffic flows will increase the risk of rail crossing incidents occurring. Management of rail crossings is undertaken by VicTrack in liaison with VicRoads and Councils. VicTrack utilises a risk management tool, known as ALCAM (Australian Level Crossing Assessment Model) to determine risk values of rail crossings which assists in prioritising rail crossing improvement works.

It is recommended that the Campaspe Shire/land developers liaise with VicTrack to have the increased risk of rail crossing incidents occurring within and near the Precinct assessed and seek appropriate improvements. This will occur as part of the development plan approval and will be the responsibility of land developers.